

# Southeast

## area structure plan



Prepared by  
City of Lethbridge  
910-4 Avenue S  
Lethbridge, AB T1J 0P6  
[www.lethbridge.ca](http://www.lethbridge.ca)



CITY OF  
*Lethbridge*



BYLAW 6000

\*\*\*\*\*

A BYLAW OF THE CITY OF LETHBRIDGE  
TO ADOPT AN AREA STRUCTURE PLAN FOR LANDS  
WITHIN THE CITY OF LETHBRIDGE

\*\*\*\*\*

WHEREAS Section 633(1) of the Municipal Government Act, Chapter M-26, R.S.A. 2000, provides as follows:

633(1) For the purpose of providing a framework for subsequent subdivision and development of an area of land, a council may, by bylaw, adopt an area structure plan.

AND WHEREAS the Council of the City of Lethbridge wishes to adopt an Area Structure Plan for those lands in the City of Lethbridge generally described as:

1. Ptn of NW ¼ 27-8-21 W4M
2. Ptn of NE ¼ 27-8-21 W4M
3. Ptn of LSD 3,4, 5 and 6, SW ¼ 27-8-21 W4M
4. Ptn of SE ¼ 27-8-21 W4M
5. Plan 0613929 Block 1 Lot 3
6. Plan 0410527 Block 1 Lot 1
7. Plan 367GJ Parcel "A"
8. Plan 8611028
9. Plan 0312980 Block 2 Lots 1 and 2
10. Plan 1457JK Lots 3 and 4
11. Plan 0311792
12. Ptn of NE ¼ 22-8-21 W4M
13. Ptn of LSD 3, 4, 5 and 6, SW ¼ 22-8-21 W4M
14. Ptn of SE ¼ 22-8-21 W4M
15. Plan 0111356 Block 2 Lot 1
16. Ptn of NW ¼ 15-8-21 W4M
17. Ptn of LSD 13, NW ¼ 15-8-21 W4M
18. Ptn of NE ¼ 15-8-21 W4M
19. Ptn of LSD 3,4, 5 and 6, SW ¼ 15-8-21 W4M
20. Plan 0914974 Block 12 Lot 10
21. Plan 9910301 Block 1
22. Plan 7452GK Blocks 3, 4 and 5
23. Plan 9811073 Block 6
24. Plan 11111181 Block 7 Lot 7
25. Plan 0411833 Block 1 Lot 2
26. Plan 0312659

27. Plan 7610425 Block E
28. Plan 9912246 Block D Lots 3, 4 and 5
29. Plan 1457JK Lots 1 and 2
30. Plan 1546GQ Blocks B and C
31. Plan 0310216 Block 1 Lots 2 and 3
32. Plan 5985GM Parcel A
33. Plan 9210487 Block 1 Lot 1

Including all Road Allowances contained within those Sections and Registered Plans, as shown on the attached Appendix "A".

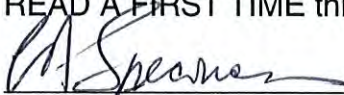
NOW THEREFORE, THE COUNCIL OF THE CITY OF LETHBRIDGE, IN THE PROVINCE OF ALBERTA, DULY ASSEMBLED, DOES HEREBY ENACT AS FOLLOWS:

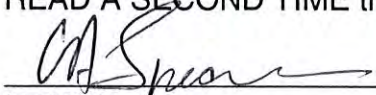
1. The Plan which is attached hereto as Appendix "B" is hereby adopted as the Area Structure Plan for that area of land in the City of Lethbridge generally described as:
  1. Ptn of NW ¼ 27-8-21 W4M
  2. Ptn of NE ¼ 27-8-21 W4M
  3. Ptn of LSD 3,4, 5 and 6, SW ¼ 27-8-21 W4M
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  14. Ptn of SE ¼ 22-8-21 W4M
  15. Plan 0111356 Block 2 Lot 1
  16. Ptn of NW ¼ 15-8-21 W4M
  17. Ptn of LSD 13, NW ¼ 15-8-21 W4M
  18. Ptn of NE ¼ 15-8-21 W4M
  19. Ptn of LSD 3,4, 5 and 6, SW ¼ 15-8-21 W4M
  20. Plan 0914974 Block 12 Lot 10
  21. Plan 9910301 Block 1
  22. Plan 7452GK Blocks 3, 4 and 5
  23. Plan 9811073 Block 6
  24. Plan 11111181 Block 7 Lot 7
  25. Plan 0411833 Block 1 Lot 2
  26. Plan 0312659

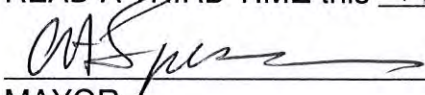
- 27. Plan 7610425 Block E
- 28. Plan 9912246 Block D Lots 3, 4 and 5
- 29. Plan 1457JK Lots 1 and 2
- 30. Plan 1546GQ Blocks B and C
- 31. Plan 0310216 Block 1 Lots 2 and 3
- 32. Plan 5985GM Parcel A
- 33. Plan 9210487 Block 1 Lot 1

Including all Road Allowances contained within those Sections and Registered Plans, as shown on the attached Appendix "A", and shall be known as the "Southeast Area Structure Plan".

- 2. This Bylaw shall come into force on the date of final passage thereof.

READ A FIRST TIME this 4<sup>TH</sup> day of APRIL, 2016  
  
 \_\_\_\_\_  
 MAYOR DEPUTY D. Sandfield  
 \_\_\_\_\_  
 CITY CLERK

READ A SECOND TIME this 16 day of May, 2016  
  
 \_\_\_\_\_  
 MAYOR DEPUTY D. Sandfield  
 \_\_\_\_\_  
 CITY CLERK

READ A THIRD TIME this 16 day of May, 2016  
  
 \_\_\_\_\_  
 MAYOR DEPUTY D. Sandfield  
 \_\_\_\_\_  
 CITY CLERK



This document entitled *Southeast Area Structure Plan* was prepared under the supervision of the undersigned Registered Professional Planner (RPP) for The City of Lethbridge – Planning and Development Services. The material in it reflects the professional judgment of the undersigned RPP and the opinions in the document are based on conditions and information existing at the time the document was published and do not take into account any subsequent changes.



ASP Document Prepared Under the  
Supervision of: **Tyson Boylan, RPP, MCIP**





## Project Team Dedication

The Southeast Area Structure Plan area is a new area in the continued growth and success of the City. As the Southeast Area Structure Plan has a long range focus, it will continue to evolve over time. The area will be a place for residents to work, play and live and will incorporate characteristics that have not been yet fully imagined.

As we have seen through the formation of this Plan, this area will be a challenge to successfully develop. This challenge has been embraced by the members of the Project Team whole-heartedly. The Project Team are confident that, moving forward with the combined commitment and effort of City Staff, developers and the landowners, these challenges can be met and overcome. At the end of the day, this will result in more resilient and appealing neighbourhoods for future employees and residents who will call the Southeast Area Structure Plan area home.

Signed,

Southeast Area Structure Plan  
Project Team  
January 2016



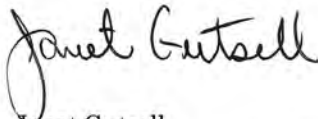
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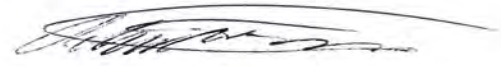
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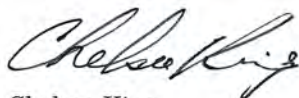
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Transportation Planning Manager



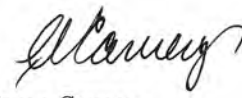
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Transportation Engineer



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Director of Planning and Development



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Technologist



Karen Carney  
Assistant to the Director of  
Infrastructure



# **Southeast Area Structure Plan**

Prepared by:

City of Lethbridge  
Planning & Development Services  
910 – 4 Avenue South  
Lethbridge, Alberta  
T1J 0P6

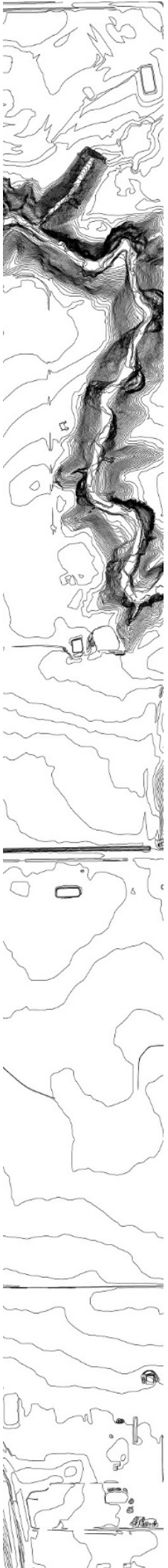
Adopted By City of Lethbridge Bylaw No. 6000, May 16, 2016



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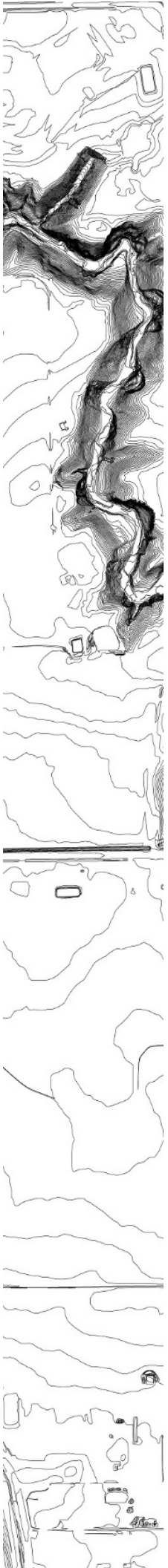




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# List of Acronyms

Acronym	Full Text
ASP	Area Structure Plan
ALSA	Alberta Land Stewardship Act
BPMP	Bikeways and Pathways Master Plan
CIP	Capital Improvement Program
CPR	Canadian Pacific Railway
CPTED	Crime Prevention Through Environmental Design
DC	Direct Control land use district
EPANET	Environmental Protection Agency NETWORK Model
EPASWMM	Environmental Protection Agency Storm Water Management Model
FUD	Future Urban Development land use district
HIRF	High Intensity Residential Fire
ICSP/MDP	Integrated Community Sustainability Plan / Municipal Development Plan
ID	Irrigation District
IDP	Intermunicipal Development Plan
LUB	Land Use Bylaw 5700
MGA	Municipal Government Act
MPC	Municipal Planning Commission
PCB	Polychlorinated Biphenyl
PHC	Petroleum Hydrocarbon
PMP	Parks Master Plan
RCMP	Recreation and Culture Master Plan
RVPMP	River Valley Parks Master Plan
SEASP	Southeast Area Structure Plan
SELUP	Southeast Lethbridge Urbanization Plan
SMRID	St. Mary River Irrigation District
SSRP	South Saskatchewan Regional Plan
TIA	Transportation Impact Assessment
TMP	Transportation Master Plan

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# 1. Introduction

## 1.1 Purpose

The purpose of this Area Structure Plan (ASP) is to provide a planning framework that will guide the long-term development and land use pattern for southeast Lethbridge, in accordance with the City of Lethbridge Integrated Community Sustainability Plan / Municipal Development Plan (ICSP/MDP). It is intended for this area of the City to primarily be developed to provide new residential development in South Lethbridge. This area will also contain commercial and business industrial land uses, in addition to public uses.

The ASP has been prepared in conformity with both the South Saskatchewan Regional Plan (SSRP) and Section 633 of the Municipal Government Act. In accordance with Section 638.1 of the Municipal Government Act, the SSRP will prevail in the event of a conflict or inconsistency between it and the ASP.

## 1.2 Justification and Background

Until recently, South Lethbridge has experienced a great amount of the City's overall residential and commercial growth. However, population growth in South Lethbridge has slowed in the past decade and small population declines in South Lethbridge were experienced in the years 2005, 2010 and 2013. This slow growth is due to an aging population in many areas of South Lethbridge, and the fact that areas available for new development are limited to only the Southgate, Sandstone, Gold Canyon and Arbour Ridge subdivisions. The most recent residential parcels in Southgate were created through subdivision in 2015. Beyond this, further urban development in Southgate is halted until new utility services are made available. Arbour Ridge, Sandstone and Gold Canyon contain homes that are on the higher end of the market. As the Lethbridge market in this segment is limited, growth in these areas is expected to be slow.

The Southeast area will contain a significant amount of residential development in order to facilitate this future demand for housing in South Lethbridge. Following the ICSP/MDP, residential development in this area will include a diversity of densities and housing types to accommodate the future needs of the community.

Due to its proximity 100km north of the United States Border and its location on the CANAMEX Trade Corridor, Lethbridge is an important trade center in Southern Alberta. This corridor is a major north-south international trade route that connects Mexico, the United States and Canada. From a regional perspective, business industrial development included in the Southeast Area Structure Plan (SEASP) area will enhance the city's prominence as a trade centre and will improve upon the city's trade abilities as a whole. This type of development will also provide additional employment in South Lethbridge to complement the existing and future employment nodes in the Downtown, Sherring Industrial Park, W.T. Hill Business Park, and the West Lethbridge Employment Centre.

The southeast area of the city was brought into the City’s current boundary through the 1984 annexation with the intention that this area would be utilized for residential and employment-generating land uses. Conceptually, the current Integrated Community Sustainability Plan / Municipal Development Plan (ICSP/MDP), which was approved in 2010, shows the SEASP area as containing primarily industrial uses north of Highway 4 and primarily residential uses south of Highway 4. Municipal Development Plans that precede the 2010 version also show these same land uses, as does the land use study that was completed for the 1984 annexation that brought the SEASP area into the City boundary.

### 1.3 Planning Principles

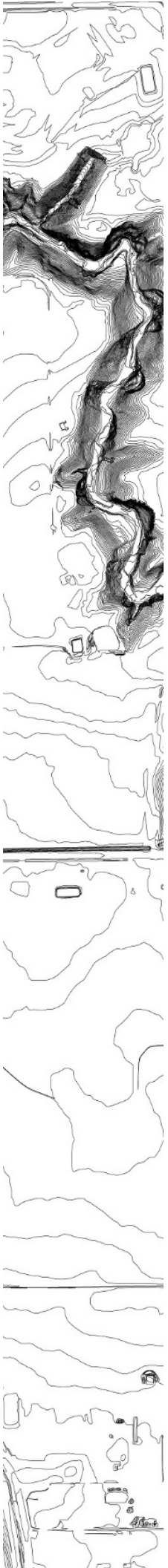
The SEASP is necessary to facilitate the continuation of growth in South Lethbridge. At the outset of the SEASP project, a number of planning principles were identified. The intention of the principles was to guide the preparation of the ASP and its future implementation through the development of a land use concept for Southeast Lethbridge. A summary of the SEASP planning principles is provided in Figure 1. A full listing of the principles are located in the Terms of Reference, found in the Technical Documents Appendix.

These planning principles are incorporated into the Objectives and Policies of the SEASP.



Planning Principles
<b>Balance</b> residential growth across Lethbridge
<b>Create</b> business and employment opportunities
<b>Minimize</b> development constraints
<b>Integrate</b> plan area with adjacent neighbourhoods
<b>Emphasize and Integrate</b> open space
<b>Incorporate</b> existing land uses
<b>Contribute</b> to local economic diversification
<b>Create</b> a dynamic, attractive City gateway
<b>Ensure</b> financially sustainable infrastructure delivery
<b>Consider</b> all transportation modes
<b>Promote</b> waste efficiency and energy efficient design
<b>Consider</b> community and market needs

Figure 1 Planning Principles



## 1.4 Plan Organization

The SEASP is organized into the following sections:

### Plan Organization

- 1 Introduction
- 2 Existing Conditions & Development Conditions
- 3 Development Potential
- 4 Land Use Concept
- 5 Park & Open Space System
- 6 Transportation System
- 7 Utility Servicing
- 8 Implementation

Figure 2 Plan Organization

## 1.5 Plan Area

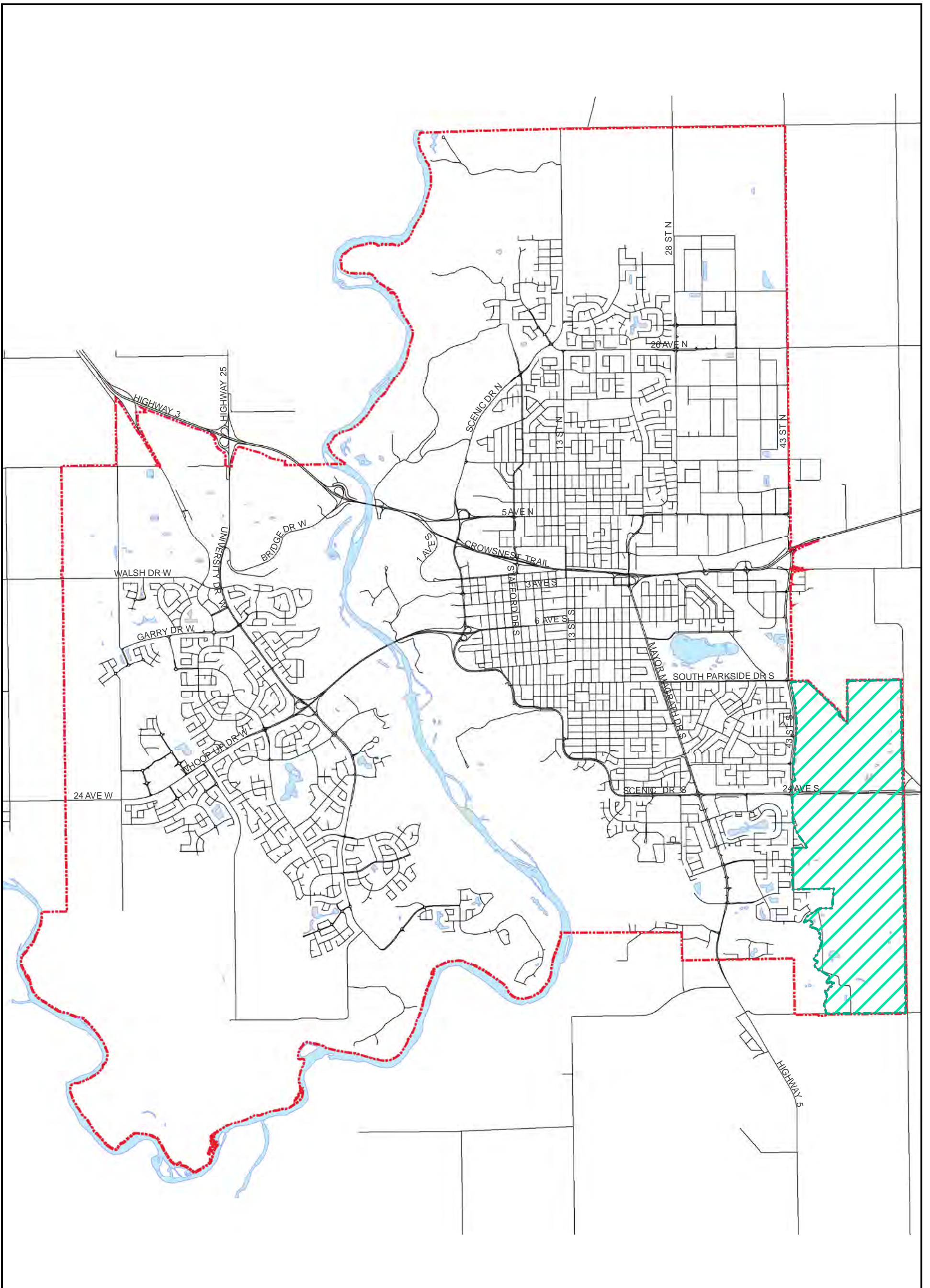
The location of the SEASP area is in the southeast corner of the city, as shown on Map 1. It is bordered by the existing city boundary with Lethbridge County to the north, east and south. To the west, the plan area is bordered by 43 Street, Six-Mile Coulee and the existing subdivisions of Fairmont and Southgate (see Map 2). There are a number of uses that currently exist in the plan area along Highway 4. Most of these uses were present prior to the 1984 annexation when this area was part of Lethbridge County. The remainder of the plan area is largely used for agricultural purposes.

Highway 4 enters the City from the east, where it becomes 24 Avenue S, and passes through the northern portion of the SEASP area in an east-west direction. The rail line that crosses the border from Montana and connects to the main line in Lethbridge also passes through the northern portion of the SEASP area in a northeast-southeast direction. Further description of the plan area is provided in Section 2.

## 1.6 Land Ownership & Parcel Size

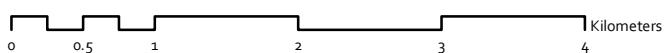
As shown on Map 3, there are approximately 50 individual, distinct property owners (or groups of owners) within the plan area. This area of South Lethbridge has been subject to some subdivision activity in the past and, as a result, the average parcel size is just over 5 hectares in size - with the largest parcel being 64 hectares, or one quarter section. This has resulted in a large base of property owners that have been involved and kept informed throughout the planning process.





### Legend

-  City Boundary
-  Southeast Area Structure Plan
-  Hydrology
-  Existing Roads



## MAP 1 - Location

The Southeast Area Structure Plan covers an area of 679 hectares in southeast Lethbridge.







### Legend



City Boundary

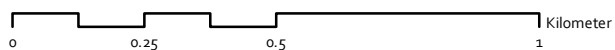


Southeast Area Structure Plan



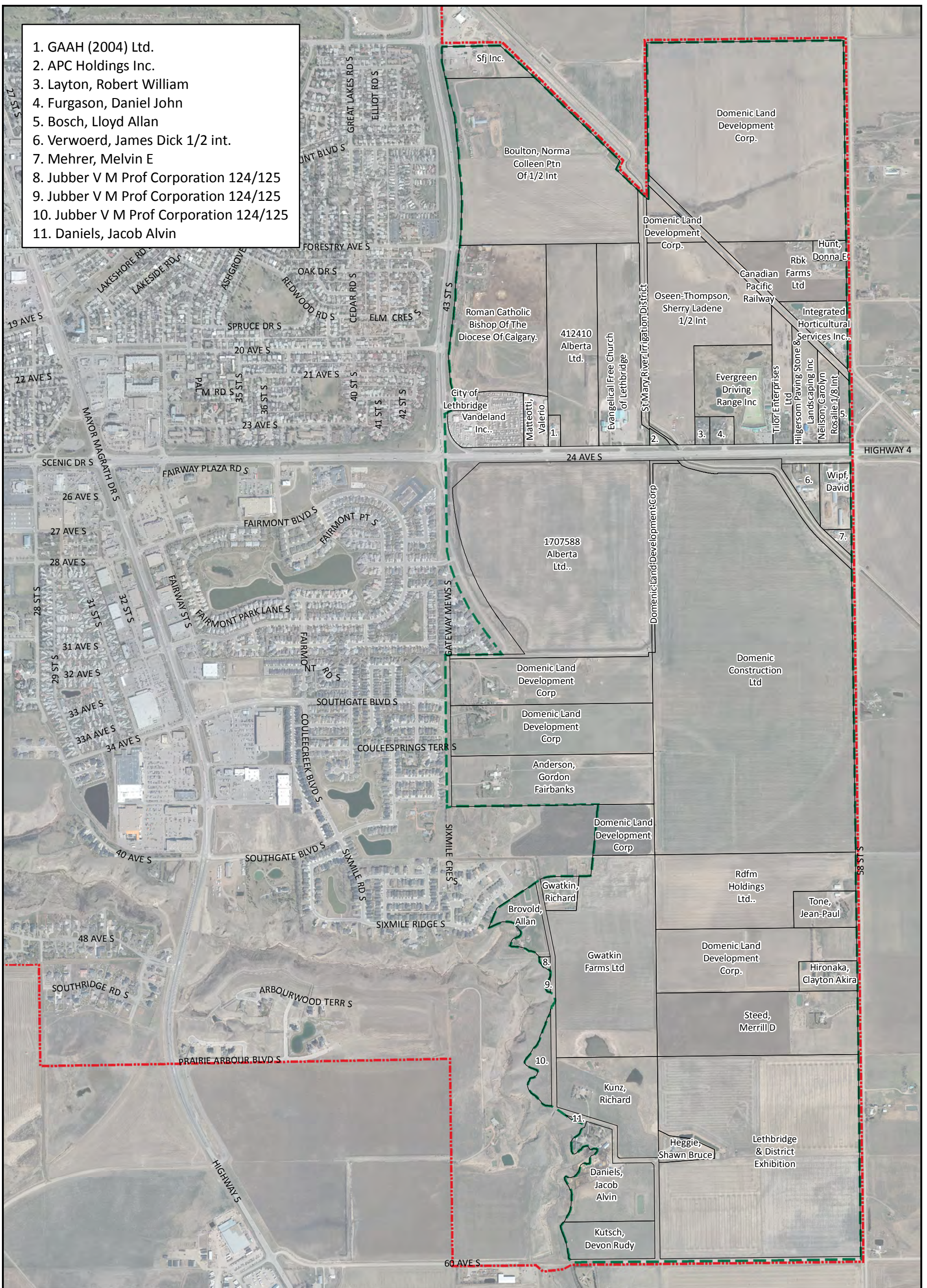
## MAP 2 - Plan Area

The plan area is bound to the east, south, and north by the City boundary; and by Six Mile Coulee, 43rd Street, and the existing neighbourhoods of Fairmont and Southgate to the west. Highway 4 enters the City from the east, where it becomes 24th Avenue South and passes through the Plan area from east to west. The railway from Montana diagonally bisects the northern portion of the plan area.








1. GAAH (2004) Ltd.
2. APC Holdings Inc.
3. Layton, Robert William
4. Furgason, Daniel John
5. Bosch, Lloyd Allan
6. Verwoerd, James Dick 1/2 int.
7. Mehrer, Melvin E
8. Jubber V M Prof Corporation 124/125
9. Jubber V M Prof Corporation 124/125
10. Jubber V M Prof Corporation 124/125
11. Daniels, Jacob Alvin



**Legend**

-  Parcel Boundary
-  City Boundary
-  Southeast Area Structure Plan

0 0.25 0.5 1 Kilometer



**Map 3 - Landowners**

Landowners in the Southeast Plan Area as of September 21, 2015.





## 1.7 Planning Process

This ASP commenced in 2013 and was completed in early 2016. The remainder of Section 1.7 summarizes the background research and community consultation that has contributed to the preparation of the plan:

### 1.7.1 Terms of Reference

The process of drafting an ASP involves many different tasks that must be completed at certain intervals and requires the incorporation of data from different technical reports. A Terms of Reference document helped the project team effectively navigate through this process by defining the major components and work schedule for the ASP. In accordance with the City of Lethbridge’s Planning Process, and prior to the commencement of an ASP, the Terms of Reference must be reviewed by the Municipal Planning Commission (MPC) for the purpose of ensuring the objectives of the ICSP/MDP are met. The SEASP Terms of Reference were approved by MPC on March 11, 2014 and are included in Appendix C.

Prior to the final draft of the ASP going to City Council for adoption, a draft ASP was required to return to MPC for their review to ensure the policy content and public consultation aspects of the ASP were in accordance with the Terms of Reference. Following this review, MPC then recommended the draft ASP be submitted to City Council for their approval. The SEASP received MPC’s recommendation on February 16, 2016 (see Appendix C for the signed MPC resolution).

### 1.7.2 Background Research & Studies

To assist in the preparation of the ASP, a number of key background and research studies were commissioned. As listed in Figure 3, these technical reports have been provided as background information in developing the policies of the ASP. As such, they do not form part of the ASP Bylaw to be adopted by City Council. The studies can be found in their entirety in the Technical Documents Appendix of the ASP.

#### Background Research & Studies

Traditional Knowledge and Use Assessment	Geotechnical Evaluation
Southeast Lethbridge Market Analysis and Development Forecast	Stormwater Strategy—Value Engineering Study
Fiscal Impact Analysis	Traffic Impact Assessment
Biophysical Impact Assessment	Utility Servicing Plan
Phase 1 Environmental Impact Assessment	24 Avenue Gateway Corridor Design Guidelines

Figure 3 Background Research & Studies



A vertical topographic map of a river valley, showing contour lines, a winding river, and some structures. The map is positioned on the left side of the page.

### 1.7.3 Community Consultation

A crucial part of any planning process is consultation with stakeholders and the community as a whole. For the SEASP, direct mailings, media releases, newspaper advertisements and notifications on the City's website were used in different capacities during the planning process to create awareness of the project and to gather stakeholder and community input.

Consultation activities that were undertaken are as follows:

**Landowner & Project Stakeholder Interviews.** During the spring of 2013, a series of meetings with landowners within the plan area were held. They were informal one-on-one sessions, at which City staff introduced the planning project, asked what individual landowners envisioned for the future of their land and answered any questions posed by the landowners. All landowners in the plan area were sent invitations to meet. City staff met with 14 groups of landowners who responded to the invitation.

Meetings and interviews were conducted with various stakeholders with interests in southeast Lethbridge. Stakeholders included groups such as school districts, emergency services, telecommunications services providers, CPR, Lethbridge County, St. Mary River Irrigation District (SMRID) and ATCO Pipelines. The purpose of these meetings was to identify stakeholder interests and assets in the region, as well as address possible opportunities and constraints through the use of targeted policies.

In addition to these formal meetings, discussions with a number of affected landowners, developers and stakeholders also took place to ensure open dialogue continued, throughout the entirety of the SEASP planning process. These meetings were organized on an as-needed basis at the request of either the interested party or project staff and interests from both north and south of 24 Avenue S (Highway 4) were represented.

**Property Owner Meetings.** On April 16, 2014 a meeting was held for property owners within the plan area. Owners were invited to the meeting through direct mailings, and representatives from 19 of the area's property owners were in attendance. Project staff made a presentation that outlined the general themes of the proposed plan and the schedule for the project. Opportunity was given for the attendees to ask questions and give feedback with regard to the SEASP.

A second property owners meeting was held on June 9, 2015, at the Enmax Centre. Property owners were again invited to this meeting through direct mailings and representatives from 29 of the area's property owners were in attendance. Poster boards were set up to describe the proposed land use concept and to describe the remaining process of bringing the ASP to City Council for a decision. Project staff were present to answer questions, gather feedback and to provide further clarification where needed. The final draft of the ASP was presented to property owners at a final meeting on October 21, 2015. Copies of the comments received at these meetings are included in the Technical Documents Appendix.



These meetings were different from the informal landowner interviews that were conducted in 2013.

**Public Open Houses.** Three Public Open House sessions were held for this project. The initial Open House, attended by approximately 80 people, was held April 30, 2014. The purpose of this initial Open House was to present the overall planning vision, goals, approach, constraints, development options and the possibility of integrating the plan area with the adjacent subdivisions. Participants were given the ability to provide comments and feedback through an exit survey. The comments from the first Open House were reviewed by project staff and considered alongside technical reports for use in drafting the SEASP.

This proposed SEASP land use concept was then presented at a second Open House held on June 10, 2015, at the Enmax Centre. Approximately 70 people attended and participants provided their comments on the draft ASP through an exit survey. As the SEASP will effect specific area residents, all Southgate residents, and residents located in the eastern portion of Fairmont, were sent direct notification for this meeting. The general public was also invited to review the initial draft of the SEASP and provide comments through the City's website, or in person at City Hall. Following the second Open House, comments received through both Open Houses were considered in further refinement of the draft SEASP.

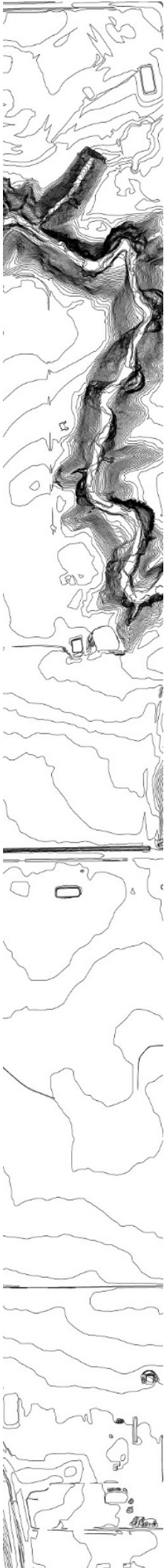


*Actual fortune cookie guidance received by a project team member the day of an open house*

The final Open House, where a final draft of the proposed ASP was presented, occurred on October 21, 2015. Copies of the comments received at all three Open Houses from the General Public, landowners and stakeholders are included in the Technical Documents Appendix.

**First Nations Engagement.** Engagement with First Nations communities, as directed by the South Saskatchewan Regional Plan (SSRP), was undertaken in the form of a formal Traditional Knowledge and Use Assessment. The Assessment is reviewed in Section 2.4.2, and can be found in Technical Documents Appendix of the ASP.





## 2.0 Existing Conditions & Development Considerations

### 2.1 Environmental Findings

#### 2.1.1 Geotechnical Evaluation

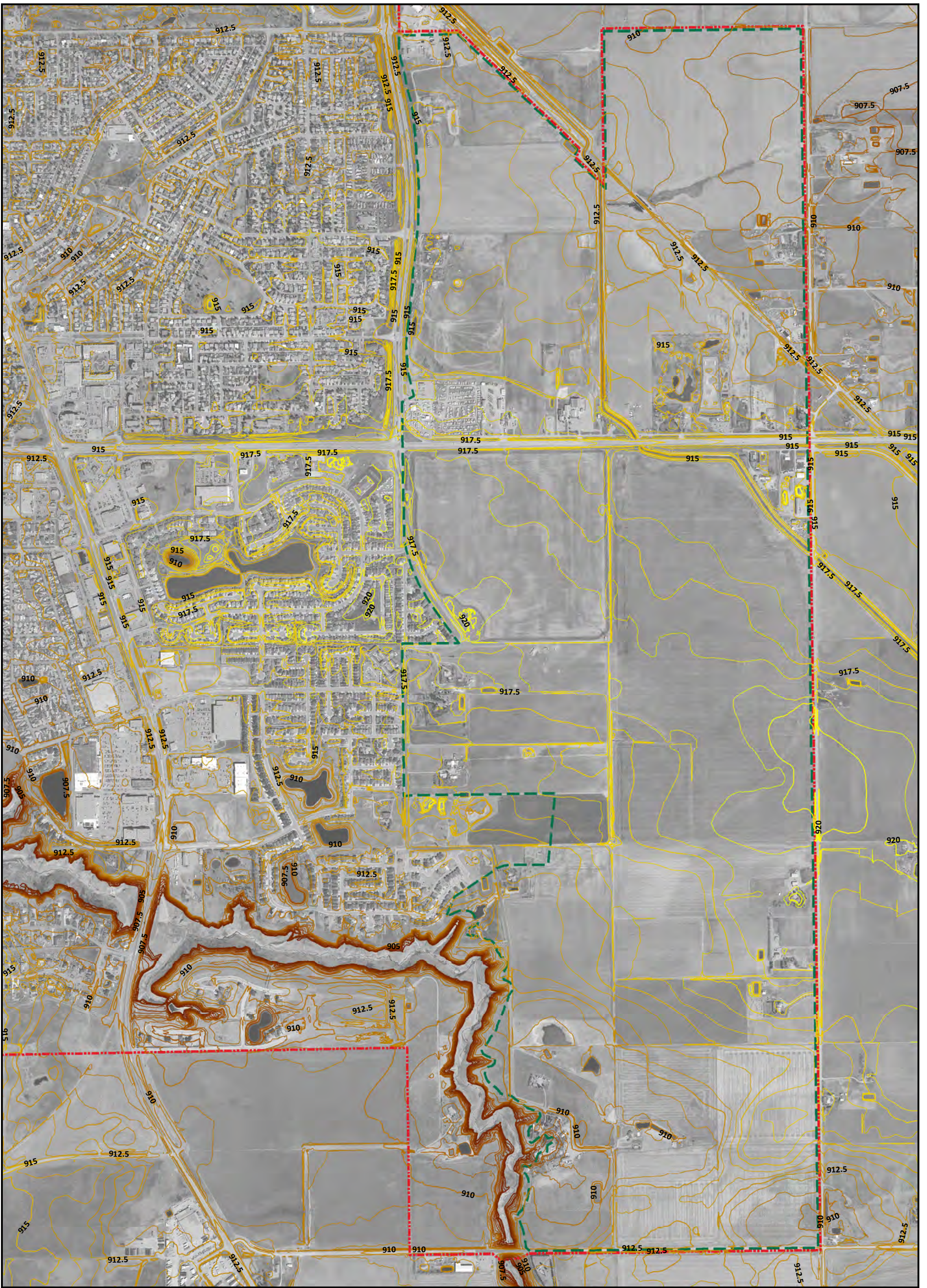
A Geotechnical Evaluation was conducted in July 2013. The Evaluation consisted of a desktop review of existing studies and publically available information. One on-site inspection was also carried-out, as well as a review of historical aerial photographs of the plan area. The scope of the Geotechnical Evaluation was to review the general subsurface conditions and provide general recommendations for design and construction, including: Foundations, stormwater management facilities, pavement, grading and lot development. More in-depth geotechnical analyses shall be required as part of future Outline Plans.

The main findings of the Evaluation indicated that the area is generally suitable for the proposed types of land uses outlined conceptually in the SEASP. Determination of specific geotechnical site suitability for individual phases of development will be required as part of each Outline Plan. The Evaluation describes the geotechnical conditions in the plan area as being consistent with the region. Caution is expressed about development in proximity to Six-Mile Coulee as well as on parcels that may contain historic irrigation canals that have been filled in and are no longer in use. Risk mitigation strategies are provided in the document with regard to these sites, as well as more generally throughout the plan area and should be referred to in subsequent Outline Plans.



For further details of the Geotechnical Evaluation and its recommendations, please refer the full document in the Technical Documents Appendix.

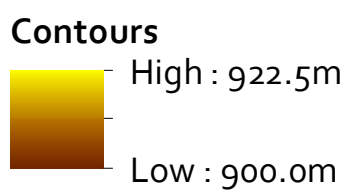
#### 2.1.2 Biophysical Impact Assessment

A Biophysical Impact Assessment was conducted in July, 2013. The Assessment consisted of a desktop review of existing data, including publications and datasets of federal, provincial and non-profit agencies. One on-site inspection was also carried-out, as well as a review of historical aerial photographs of the plan



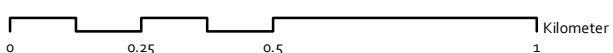
### Legend

-  City Boundary
-  Southeast Area Structure Plan



## MAP 4 - Topography

The topography of the Southeast plan area is relatively flat, with some hummocky regions. The elevation within the plan area ranges from 905.5m to 926m, decreasing in elevation in the south towards the banks of Six Mile Coulee and northeast towards the city boundary.





area. The scope of the Biophysical Impact Assessment was to identify sensitive biological and physical features found within the plan area that may act as constraints to future development. The Assessment also provides mitigation measures and recommendations concerning additional site-specific studies.

The Assessment found that there are four principal land uses within the plan area: Lands under cultivation (approximately 70%), Pasture lands (approximately 18%), Developed lands (approximately 12%) and Waterbodies and Wetlands (approximately 0.5%). The plan area coincides with the habitat range of a number of federal and provincial species of concern. However, given the largely disturbed nature of the plan area, the likelihood of occurrence of any species of concern is considered low. The Assessment recommends that prior to development, additional analysis be performed to classify wetlands and determine the potential presence of avian and amphibian species. Moreover, there are regulatory limitations on the time of the year that field investigation can occur.

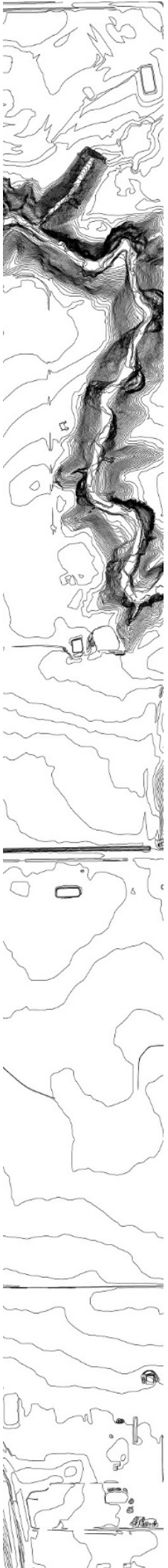
For further details of the Biophysical Impact Assessment and its recommendations, please refer the full document in the Technical Documents Appendix.

#### 2.1.3 Phase 1 Environmental Site Assessment

A Phase 1 Environmental Site Assessment was conducted in July 2013. The Assessment consisted of a review of public records pertaining to historical land use, including those found at the City of Lethbridge, Lethbridge County and the Galt Archives. One on-site inspection was also carried-out as well as a review of historical aerial photographs of the plan area. The scope of the Phase 1 Environmental Site Assessment was to review past and present land uses both within and having external influence upon the plan area. The review assessed the potential for these land uses to environmentally impair the plan area and thereby place constraints upon potential development. The Assessment also provides recommendations concerning site remediation and additional site-specific studies within the plan area.

The Assessment identified environmental features relevant to the entire plan area that will require consideration as to how these features will be mitigated or integrated into the development concept. These features include: Farm/acreage yards, Dugouts, Wetlands and / or Historic canals and Site buildings. The Assessment also provides additional detail about potential hazards within three defined sub-areas of analysis - North Lands, Middle Lands and South Lands that correspond with approximately three Sections of land contained within the plan area.





In total, ten sites within the plan area were identified due to historical and / or current land uses that represent potential environmental concerns. Key potential environmental concerns identified include:

- Potential impact on soil and groundwater from sites of former and current Petroleum Hydrocarbon (PHC) handling and storage.
- Potential impact on soil and groundwater from sites of former and current livestock operations from chloride and other nutrients.
- Potential impacts on soil and groundwater from former and current vehicle storage, including glycols, PHCs and metals.
- Potential impairment from well site and pipelines, due to drilling and construction activities.

In general, there do not appear to be any significant environmental constraints that would necessarily prevent the future development of this plan area as conceptualized in the ASP. That being said, the Assessment conducted is broadly-scoped and future site-specific intrusive environmental analysis will be required as part of each Outline Plan. Future analysis will determine the exact nature and severity of any potential environmental impairments identified in the Phase 1 Assessment. Such future study may include, but is not limited to, a Phase 2 Environmental Site Assessment and should consider recommendations to properly remediate and fill in dugouts, so they can properly be developed.

For further details of the Phase 1 Environmental Site Assessment and its recommendations, please refer to the full document in the Technical Documents Appendix.

#### 2.1.4 Objectives

- a) Confirm the intended development is appropriate from geotechnical, biophysical, environmental and traditional land use perspectives.

#### 2.1.5 Policies

- a) Additional Geotechnical investigation shall be undertaken at the Outline Plan stage to verify that the Outline Plan is appropriate for the intended land uses, as stated in the Geotechnical Evaluation. This shall include a borehole sampling program where needed.
- b) Additional Biophysical investigation shall be undertaken at the Outline Plan stage to verify that the Outline Plan is appropriate for the intended land uses, as stated in the Biophysical Impact Analysis.
- c) Additional Biophysical investigation undertaken at the Outline Plan stage for Outline Plan areas R1b and R3 (see Map 15) shall survey Six Mile Coulee for snake hibernacula.

Mitigation of any snake hibernacula would require an appropriate buffer from development that is consistent with provincial regulations.

- d) Additional Biophysical field investigation conducted at the Outline Plan stage shall occur in the springtime to best determine what animal species will be impacted by construction, define appropriate mitigation measures and to avoid nesting/breeding times.
- e) Additional Environmental investigation shall be undertaken at the Outline Plan stage to verify that the Outline Plan is appropriate for the intended land uses, as stated in the Phase 1 Environmental Impact Assessment.
- f) Treatment of sensitive species discovered or encountered shall be in accordance with provincial legislation and federal legislation (i.e. Species at Risk Act, Alberta Wildlife Act).

## 2.2 Existing Uses

The majority of land within the plan area is currently cultivated for agricultural purposes. There are farmsteads and small acreages scattered throughout the area. The majority of commercial development is located adjacent to Highway 4 and includes an RV dealership, animal hospital, church, golf course, as well as landscaping and excavating companies (See Map 5).

Almost all development in the plan area was established prior to annexation by the City in 1984. Development in existence prior to annexation has been permitted to expand and change uses (within the requirements of their land use district) since that time.

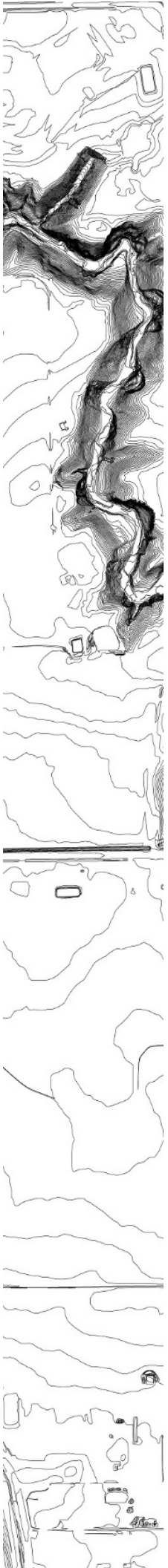
For the most part, parcels contained within the plan area are zoned as the Future Urban Development (FUD) land use district or as a Direct Control (DC) land use district. The FUD district is the standard district used by the City for lands that are largely undeveloped and allows the existence of rural or agricultural-type uses. Once land is ready for development and servicing, it is rezoned to a more appropriate district as part of the City's planning process.

The DC district is a site specific land use district that is used in situations where a standard land use district would not provide a desired form of development or would not provide any restrictions that may be necessary. Within the SEASP area, DC districts have often been



*Existing developments in the SEASP area*





utilized to permit the continued operation of existing development (while restricting any additional development), until the area is ready for urban development. For example, the DC land use district for the existing church allows the continued expansion of the church and related activities, but does not allow for the development of a service station on the undeveloped portion of its site. DC districts have also been used to allow large parcels of land (i.e. over 20 hectares in size) to be subdivided once, allowing land owners to sell an underutilized portion of their land (while restricting significant development), until sufficient planning is completed and utility services are available.

As the southwestern portion of the plan area was previously under influence of the Southgate Area Structure Plan and Outline Plan, a segment of this area has been previously rezoned to the Low Density Residential (R-L) district and is currently under development. The SEASP will provide for the integration of this area with any future development to the east.

Existing roadways include Highway 4 (24 Avenue S), 43 Street S and 60 Avenue S. The existing 58 Street alignment is located in Lethbridge County adjacent to the east of the plan area and currently serves residents of both the County and City. Connections between existing communities to the west and the plan area will be developed or upgraded as future development proceeds.

There are a number of major private utilities within the plan area including telecommunications, irrigation, electrical and railroad infrastructures. These are discussed in greater detail in Section 2.3- Development Constraints.

### 2.3 Development Constraints

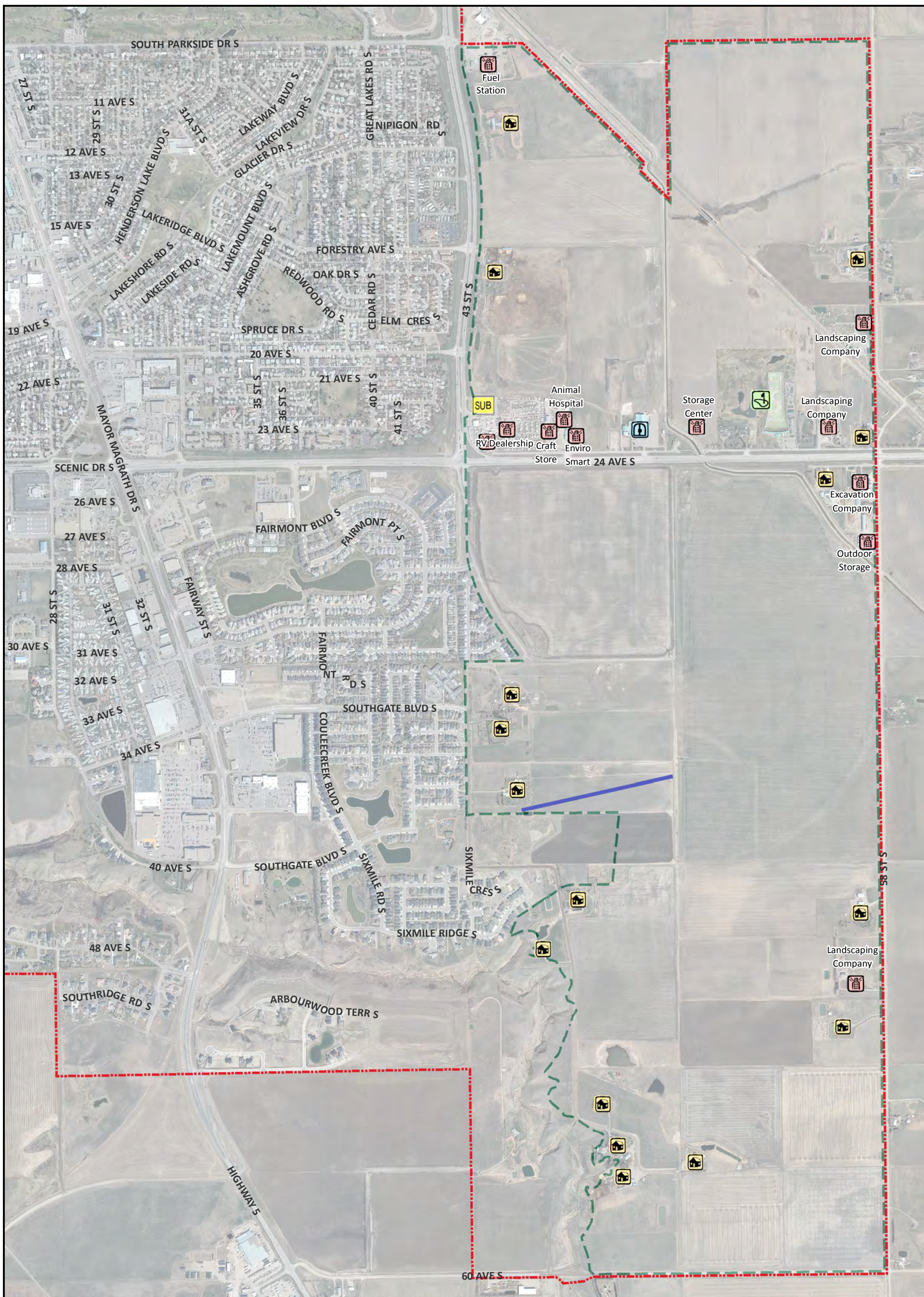
There are a number of existing development constraints within the SEASP area. These constraints were considered during the preparation of the SEASP, and are shown on Map 6.

**Stormwater.** The management of stormwater is the most notable constraint in south east Lethbridge and will be the most critical to successfully mitigate. The terrain of the SEASP area is low relative to adjacent developed areas of the City, with a ridge running diagonally northwest to southeast through the approximate middle of the plan area. The land on the northern side of the ridge naturally drains toward the east and northeast, away from the rest of the City, in the direction of Highway 4 and Lethbridge County. Lands to the south of the ridge drain toward the adjacent neighbourhoods and Six-Mile Coulee.



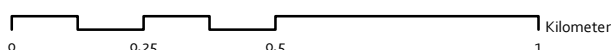
*Stormwater concerns within the SEASP area*





### Legend

-  City Boundary
-  Southeast Area Structure Plan
-  Air Strip
-  Electrical Substation
-  Business
-  Private Dwelling
-  Recreation
-  Religious Assembly

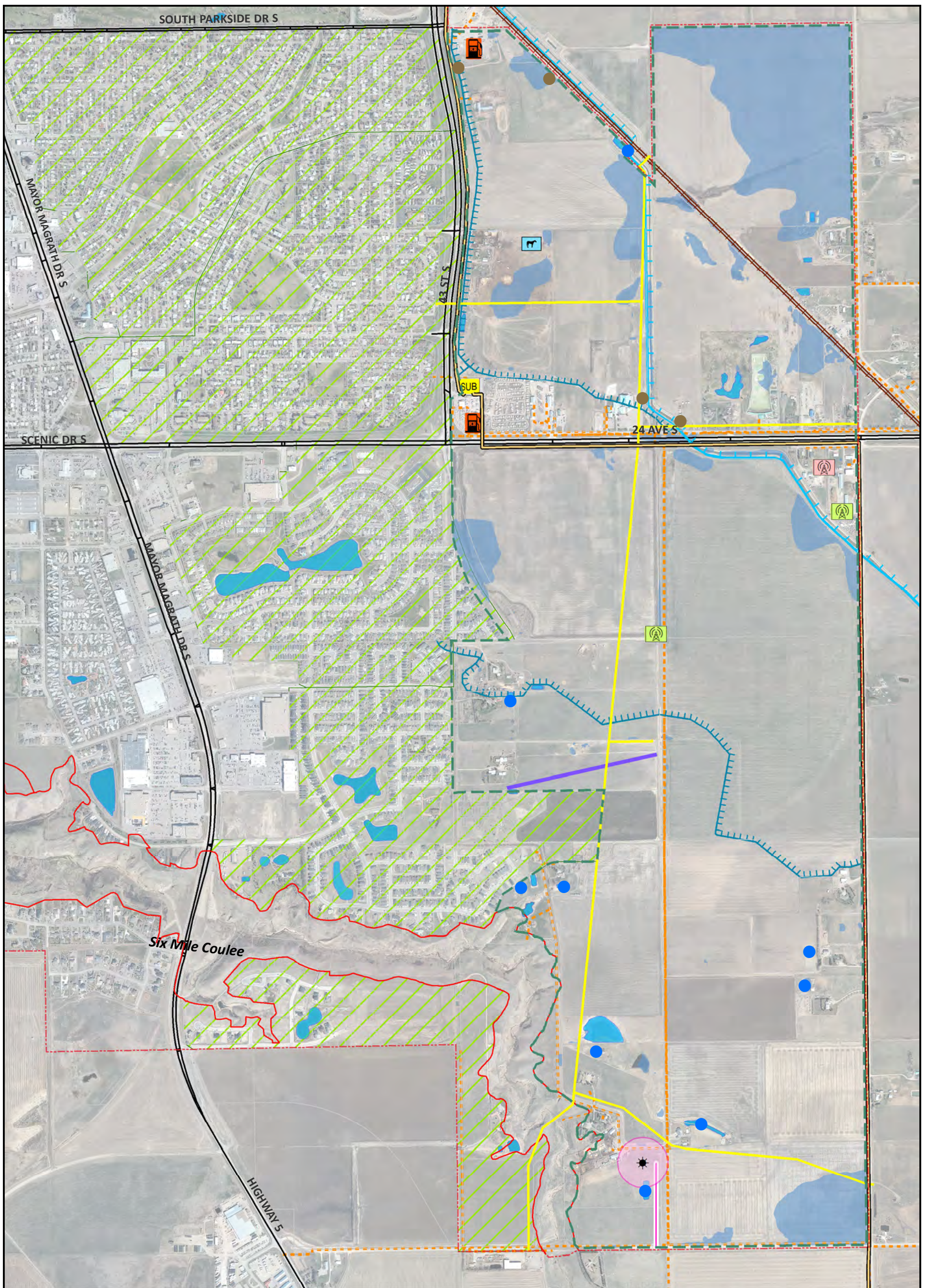


## MAP 5 - Existing Uses

The majority of the Southeast plan area has been cultivated for agricultural purposes, and also contains acreages, farmsteads, and businesses. There are a few developed uses present along Highway 4 such as a golf center, an animal hospital, RV retailer, craft store, storage center, and various landscaping companies. There is an electrical substation in the northern portion of the plan area and a private airstrip in the western part of the plan area.

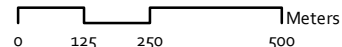






**Legend**

- |  |                                       |                               |
|--|---------------------------------------|-------------------------------|
| City Boundary                          | Former Corral                         | Former Dugout                 |
| Southeast Area Structure Plan          | Former and Existing Fuelling Stations | Active Dugout                 |
| Existing and Planned Residential Areas | Cell Tower                            | 138 kV Transmission Line      |
| Hydrology                              | Future Cell Tower                     | ATCO HP Gas                   |
| Natural Low Spots                      | Electrical Substation                 | High Pressure Production Line |
| Railway                                | Gas Well                              | SMRID Irrigation Canal        |
| Safe Development Setback Line          | 100m Buffer Around Gas Well           | Former Irrigation Canal       |
| Existing Arterial Roadway              |                                       | Telecommunication Line        |
| Air Strip                              |                                       |                               |



# MAP 6 - Development Considerations

There are a number of development constraints present in the plan area, including natural low spots, utility infrastructure, adjacent neighbourhood development, an air strip, and arterial roadways.





### Electrical Transmission Line and Substation.

A 138kV transmission line travels through the plan area, following the municipal border to the south and east of the plan area and then along Highway 4 (24 Avenue S) to the substation located near the intersection of 43 Street S and Highway 4 (24 Avenue S). Another transmission line also runs to this substation, but on a portion of the western boundary of the plan area, (parallel to 43 Street S). These transmission lines are located within the road right-of-way, typically set one meter into the right-of-way from the boundary of a parcel. A 10 meter setback (perpendicular from the center of the transmission line in both directions) is typically required to protect the transmission line from nearby development. Where new lines are installed, this setback is typically secured as an easement. However, with regard to older lines such as this, there are no title restrictions in place. The SEASP incorporates the 10m setback as much as possible into the open space concept by locating pathways, public parks and open space near transmission infrastructure. This maximizes the utility of parcels located along the electrical transmission line corridor.

It is intended for the electrical substation to remain in its current location indefinitely.

**Railway.** The alignment of the existing railway line bisects the furthest northwest portion of the plan area. The railway right-of-way consumes approximately 3.5 ha of land and physically separates an area of approximately 74 ha from the rest of the plan area to the south. This location hinders the extension of sewer and stormwater to the north side of the rail right-of-way and also limits the amount of integration and connectivity between lands on both sides of the railway tracks. Site drainage is also a concern as the line impedes the natural drainage of the northeastern plan area towards the north east.

Through the planning process several comments were received indicating a desire to relocate the railway outside of the City. At this time, the railway company and the various levels of government have no formal

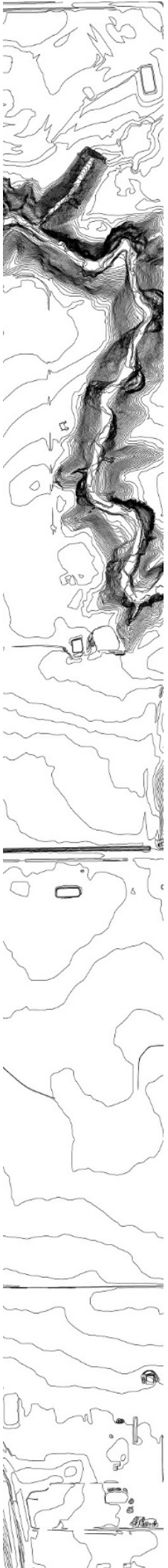


*138kV transmission line through SEASP area*



*Existing railway*





plans to move this portion of the railway from its current location. However, if this changes and this portion of the railway is relocated at some point in the future, the SEASP and its policies may be re-evaluated, as any railway relocation has the potential to impact the land use pattern, road network and utility servicing that are envisioned by this plan.

**Existing and Future Adjacent Land Uses.** The majority of the developed lands to the west of the plan area are residential in nature, with a mix of highway commercial uses running along the 24 Avenue S corridor. Lands in the County to the north, east and south of the plan area are mostly agricultural and include a number of homesteads. The SEASP will see a natural continuation of residential land uses moving east from existing neighbourhoods. An appropriate transitional area, that is sensitive to the existing agricultural lands, beyond the plan area will be conceptualized in future Outline Plans.

In particular, the Southgate neighbourhood will serve as an important transition function between neighbourhoods to the west and adjacent lands considered by the SEASP. This will include moving from a more curvilinear road network within existing developments to a modified grid road network in areas of future development.

**Irrigation Infrastructure.** The St. Mary River Irrigation District (SMRID) operates a major irrigation canal that traverses the north portion of the plan area in a southeast to north direction. Two former irrigation canals also bisect the plan area running towards the southeast, more-or-less parallel to the operational canal.

Innovative approaches to incorporating the irrigation infrastructure into the fabric of the SEASP area are explored in Sections 4 and 5. However, it is not possible to move this canal from its current alignment or place it in pipes underground. The size and importance of this piece of infrastructure makes neither of these options feasible or economical.



*SMRID irrigation canal through SEASP area*

**Hydrocarbon Contamination.** An existing card-lock fuel station is located in the far north of the plan area. This station uses underground fuel storage and will need to be adequately remediated to provincial environmental standards when or if it is redeveloped. The Environmental Overview also found the presence of a former fuel station on the northeast corner of 43 Street S and 24 Avenue S. This station also used underground storage tanks and it remains unknown as to whether this entire site was properly remediated or if the remediation would meet current environmental standards. In addition to underground storage tanks, it is also possible that hydrocarbon contamination on these sites could come from a number of sources, such as service bays or used oil storage.

Environmental studies undertaken at the Outline Plan stage for Outline Plan Area E1 will include soil and groundwater sampling for areas where underground storage tanks and/or fuel stations have existed, but are no longer present. If soil and groundwater at these sites is contaminated by hydrocarbons, remediation to the satisfaction of provincial regulations shall occur prior to development.

**Dugouts, Former Canals and Septic Systems.** As the area contains a number of existing uses that have not previously been served by water and sanitary sewer servicing, the plan area contains a number of dugouts and septic systems. As the area is developed and as utility services are extended, these dugouts and septic systems will be decommissioned by the developer.

Environmental studies undertaken at the Outline Plan stage will identify the specific location of existing and former dugouts, canals and septic systems and identify proper decommissioning and remediation methods for these facilities as they are redeveloped.

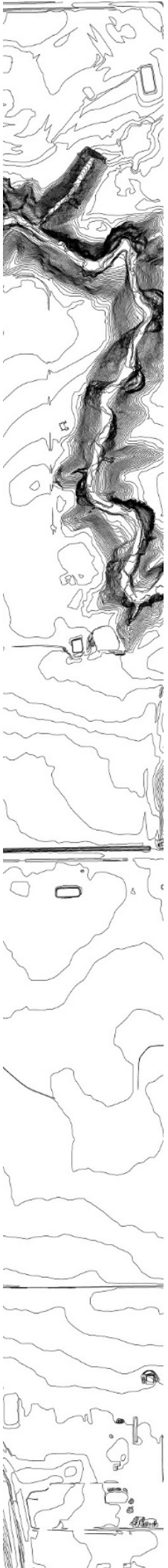
A mobile home park existed near the intersection of 24 Avenue S and 43 Street S, but the site is now used as a recreational vehicle dealership. It is unknown whether the septic system for this mobile home park was properly decommissioned and remediated, however, correspondence during the 1980's indicates that the septic system was deficient and caused a number of problems. The environmental study undertaken for Outline Plan Area E1 will identify if this septic system was properly decommissioned and remediated. If this site has not been properly decommissioned and remediated, this environmental study will determine what will be required to do so to meet current environmental standards.

**Oil and Gas Infrastructure.** There is one existing gas well in the southwest corner of the plan area (Legal Parcel Address: LSD 6 SW 15-8-21 W4M) that is currently in production. The legislated required setback for structures around a gas well head producing sweet gas (i.e. gas with no significant amounts of Hydrogen Sulphide) is currently 100 meters (see Map 6). A potential reduction in this setback would require approval from the responsible provincial authority, which is currently the Alberta Energy Regulator (AER). There is also a high pressure production pipeline that extends from this well to the south. Due to the location of the gas well and production pipeline, it is possible that the well and the pipeline will be abandoned by the time the area around it is ready for development.

There is also an existing high-pressure transmission gas line that runs northeast to southwest through most of the plan area. This pipeline is of regional importance as it serves municipalities to the south of Lethbridge, such as the towns of Raymond and Magrath. Appropriate setbacks will be incorporated into the detailed neighbourhood design at the Outline Plan stage. All natural gas facilities in the area are shown on Map 6.

Within a distance of 3.2 kilometres outside of the plan area there are twelve active sweet gas wells, eight gas wells that have been abandoned or not currently in use and numerous gas





pipelines connecting these wells. This energy infrastructure, which is located external to the SEASP area, does not impact any future development in the City, as this infrastructure would be located considerably outside of any required development setbacks.

**Highway 4.** Highway 4 (24 Avenue S) is a divided multilane highway that enters the City from the east, serving the City and region as the main connection to the US border crossing at Coutts / Sweet Grass. It will also serve as one of Lethbridge's connections to the future Highway 4 / Highway 3 bypass on CANAMEX corridor, which will be located a few kilometers to the east. Within the plan area, Highway 4 is crossed by the SMRID irrigation canal, the 138kV transmission line, telecommunications lines and a high pressure gas line.

The only significant intersection along Highway 4 is at 43 Street S. 43 Street S is the main arterial that borders the northwest portion of the plan area. Despite splitting the plan area, the high traffic volumes on Highway 4, and its role as a gateway into the City, can also be seen as a positive factor for existing and future commercial uses along this corridor.

**Air Strip.** An existing air strip (also known as an aerodrome) is located within the plan area at 3605–43 Street S (LSD 3&4 SW 22-8-21 W4M). The air strip is registered with NavCanada and appears in the Canada Flight Supplement as “CLA5”. The air strip poses limited constraints on future development. According to Federal regulations, there are no building or development restrictions placed on adjacent properties beyond those within the purview of the City of Lethbridge. Transport Canada produces guidelines for development in proximity to aerodromes, known as TP 312, however, adjacent landowners have no responsibility to comply with them. It is the responsibility of the aerodrome operator to operate the facility in a safe manner. Prior to development proceeding on this parcel, the aerodrome will be formally closed and NavCanada (or the responsible agency at the time) will be informed that it can be removed from the Canada Flight Supplement.

**Telecommunications Infrastructure.** Currently, Rogers Communications operates one telecommunications cell tower at NW 22-8-21 W4M (2–4310 24 Avenue S). The operator intends to remove this existing tower and replace it with a 79m telecommunications tower south of Highway 4 and immediately north of the operational irrigation canal at 5620 24 Avenue S. The transition from the existing tower to the new tower is scheduled to take place in March, 2016. A second telecommunications cell tower, operated by Telus, is also currently present and located near 5620 24 Avenue S.

**Six-Mile Coulee.** Six-Mile Coulee runs adjacent to the southern portion of the plan area. As such, the safe development setback line forms part of the western plan boundary. This setback line has been established throughout the City to restrict development in areas that pose a high risk of slumping into nearby coulees. The Geotechnical Study that was completed for the SEASP identified areas in the SEASP area where the top of the Six-Mile Coulee bank has been altered and recommend further site specific studies be conducted in relation to this area.



The Geotechnical studies conducted at the Outline Plan stage for Outline Plan Areas R1b and R3 will need to consider this issue in greater detail and include on-site borehole sampling in their investigation to further define the safe development setback line.

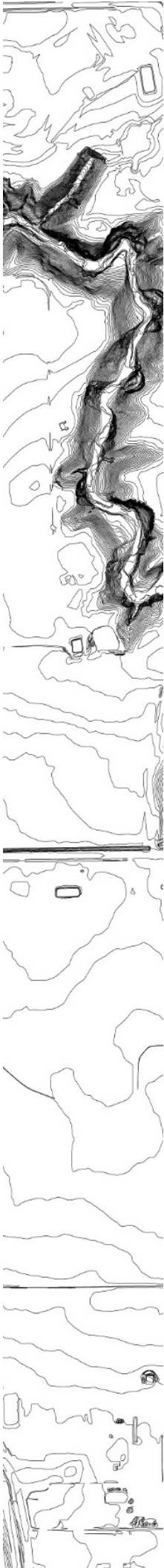
### 2.3.1 Objectives

- a) Ensure that existing and potential development constraints are fully understood and mitigated.

### 2.3.2 Policies

- a) Development adjacent to the 138kV transmission line shall be setback a minimum of 10 meters from the center of the transmission line pole.
- b) The SMRID irrigation infrastructure shall be incorporated as part of the open space system (See Section 5).
- c) Future road and pathway crossings of the SMRID irrigation canal shall be designed and constructed with input from the SMRID, so that they do not permanently impede the functioning or flow of this canal.
- d) The SMRID shall retain the ability to access its irrigation infrastructure throughout the plan area. Access to the irrigation infrastructure shall be considered during the preparation of subsequent Outline Plans.
- e) Environmental studies undertaken at the Outline Plan stage for Outline Plan Area E1 will include soil and groundwater sampling for areas where underground fuel storage tanks and/or fuel stations have existed, but are no longer present. If soil and groundwater at these sites is contaminated by hydrocarbons, remediation to the satisfaction of provincial regulations shall occur prior to development.
- f) Environmental studies undertaken at the Outline Plan stage will identify the specific location of existing and former dugouts, canals and septic systems and will identify proper decommissioning and remediation methods for these facilities as they are redeveloped. If soil and groundwater at these locations is contaminated, remediation to the satisfaction of provincial regulations shall occur prior to development.
- g) Development shall adhere to the provincially legislated setbacks from active and abandoned natural gas wells and pipelines.
- h) Any alterations to the alignment of the natural gas transmission pipeline to facilitate urban development shall absolve the City of Lethbridge from any related expenses. Any alterations are subject to the right of way agreement between the pipeline owner/operator and the landowner, and the current franchise agreement between the City of Lethbridge and ATCO Pipelines.



- 
- i) If the portion of the railway within the SEASP area is relocated in the future, the SEASP may be re-evaluated in terms of the land use pattern, road network and utility servicing for areas that are affected by a potential railway relocation.
  - j) Prior to development proceeding on the parcel containing the registered aerodrome located 3605–43 Street S (LSD 3&4 SW 22-8-21 W4M), the aerodrome shall be formally closed and removed from the Canada Flight Supplement.
  - k) Geotechnical studies undertaken at the Outline Plan stage for Outline Plan Areas R1b and R3 (see Map 15 of this document) will be required to include on-site borehole sampling around the bank of Six-Mile Coulee to further define the safe development setback line in relation to Six-Mile Coulee.

## 2.4 Historical Resources

### 2.4.1 Historical Resources Act

*Historical Resources Act* clearance for the Southeast Area Structure Plan was granted by Alberta Culture on June 11, 2013, under Section 31 of the *Historical Resources Act*. With that clearance, no formal Historical Resources Impact Assessment for either archaeological or paleontological resources was required under provincial legislation. A copy of the letter granting *Historical Resources Act* clearance can be found in the Technical Documents Appendix.

Regardless of Clearance being granted - if, at any point during the future study or development of the plan area, any historic resources are discovered, Alberta Culture must be notified. In such an event, Alberta Culture will determine what, if any, additional assessment may be required under the *Historical Resources Act*.

### 2.4.2 Traditional Knowledge & Land Use

A Traditional Knowledge and Use Assessment was conducted in early 2015 in consultation with Elders from the Kainai Nation (Blood Tribe).

This Assessment was considered an important and symbolic act by the City of Lethbridge in recognition of our respect for our First Nations neighbours and the important role that Six-Mile Coulee and the larger Oldman River valley system play within the Blackfoot traditional land use narrative. Conducting this Assessment also follows the guidance of the South Saskatchewan Regional Plan in encouraging enhanced participation of First Nations in Land Use Planning.

The Assessment consisted of a physical examination of the project area, review of maps and historical images that include the area, research of archival material and literature regarding First Nations history of the area, and discussion and consultation with knowledgeable Elders and traditional use experts. The scope of the Assessment was to assess biogeophysical conditions within the plan area that are relevant to Blackfoot peoples, including but not limited to sites of, or relating to human activity and the presence of traditional resources such as culturally significant animal and plant materials. The review assessed the potential for proposed land uses within the SEASP to impact, and to be impacted upon by, traditional use sites. Given the general and long-term disturbance of much of the plan area through cultivation, and the land use concept presented in the SEASP, the likelihood for conflict between development and traditional use sites was determined to be low. The Traditional Knowledge and Use Assessment also includes a set of recommendations that were considered by relevant parties during the preparation of the SEASP and will inform the subsequent preparation of Outline Plans.

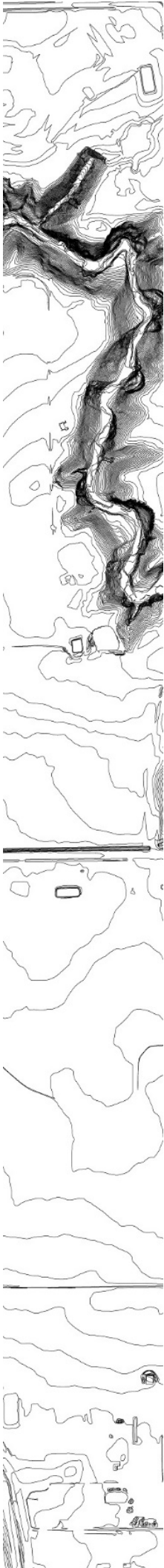


*The significance of Six-Mile Coulee includes unobstructed views of Chief Mountain (pictured) and access to traditional plants.*

Recommendations include:

- a) Include signage, particularly along Six Mile Coulee and park & open spaces, noting the area's traditional importance and present educational function.
- b) Consideration for naming streets and other features that reflect Blackfoot traditional resources and land uses in the area. A working committee should be established with Blackfoot representation to consider this option.
- c) The City of Lethbridge organize and sponsor an onsite ceremony either during an official function related to the commencement of the development in the plan area, or at the opening of any public space or feature. The ceremony should recognize the history and traditions of the Blackfoot peoples. Planning for any such future ceremony should include representation from the Blackfoot peoples.
- d) Consideration should be given to conducting a vegetation study in Six-Mile Coulee adjacent to the plan area. This information could form part of the interpretive signage suggested in Recommendation a).
- e) That a feature, sign or other element be developed and placed within the plan area recognizing Blackfoot culture. A working committee should be established with Blackfoot representation to examine this possibility.
- f) Blackfoot traditional knowledge and practices emphasize respect for the land, its plants, animals and other elements and is, in fact, a central tenet of Blackfoot culture. Any





signage or elements recognizing Blackfoot peoples and their cultural history should reflect this basic premise of respect and the need for sustaining the land and its resources.

#### 2.4.3 City of Lethbridge Heritage Inventory Sites

The City of Lethbridge Heritage Inventory is a listing of heritage sites within the City that have been identified as a potential Municipal Historic Resources. Designation does not signify expropriation, rather it is a tool municipalities use to ensure significant places are preserved for future generations. Municipal Historic Designations take place through a special bylaw process described in *Historic Resources Act*.

Within the SEASP area, one site has been identified through the Heritage Inventory. The “Parry Farm”, located at 3325 43 Street S (LSD 5&6, 22-8-21-W4M), includes a two and one-half storey house built in 1912 and a 40 acre yard formerly belonging to Lethbridge pioneer rancher and cowboy Charles Edward Parry. This is also the house where Mr. Parry’s daughter and Lethbridge’s first female Alderman, Lillian Parry, grew up. The house and yard are located adjacent to the current easterly expansion of the Fairmont neighbourhood. The site has been listed in the Heritage Inventory for its unique construction style (built with double concrete walls when most farmhouses in this period were wood frame construction) and for the personal legacy of Mr. Parry and his contributions to the Lethbridge civic and agricultural community. The full statement of significance for the site can be found in the Technical Documents Appendix.



Parry Farm and Garden  
Source: Leo and Agnes Davidson

While this site has been identified as a Heritage Inventory Site at the present time, it has not been designated as a Municipal Historic Resource.

#### 2.4.4 First Special Service Force Memorial Highway

In honour of the First Special Service Force – famously nicknamed The Devil’s Brigade or The Black Devils - and their achievements in World War II, the sections of Highway 4 and Interstate 15 from Lethbridge to Helena, Montana, were named the First Special Service Force Memorial Highway in 1999. The First Special Service Force (FSSF) was a joint Canadian-American elite commando unit of approximately 1,800 members. Up to 769 of these members were Canadian, including Canada’s most decorated soldier of First Nations descent—Tommy Prince.



*First Special Service Force commemorative plaque, Lethbridge*

The FSSF is considered to be the original special operations unit in the Canadian and American militaries and undertook extensive training in Helena. The unit conducted operations in the Aleutian Islands, Italy and Southern France prior to being disbanded in 1944. It undertook arduous missions that took on significant casualties, but resulted in the FSSF having never failed in achieving an objective and earning the respect of both the allied and enemy forces. The FSSF received a total of over 5,100 medals and decorations including, 5 Legions of Merit, 121 Silver Star Medals, and 140 Bronze Star Medals. In 2015 the FSSF was awarded the Congressional Gold Medal which (along with the Presidential Medal of Freedom), is considered the highest civilian award in the United States.

The current route of Highway 4 and Interstate 15 acted as an important corridor for the Canadian soldiers of the FSSF heading to Fort William Henry Harrison in Helena to begin their training. It is a representation of the close relationship and alliance between Canada and the United States that exists to this day. As this route begins in the SEASP, the FSSF and their history should be recognized in the plan area. This can include the naming of certain parks, streets or other landmarks after the FSSF, their members or key battles. This can also include the establishment of a monument, gateway feature or interpretative board adjacent to Highway 4 at the entrance to the city in the far east of the plan area. This is further discussed in Section 4.5.



#### 2.4.5 Red Coat Trail

The portion of Highway 4 that runs through the plan area is also part of the Red Coat Trail. The Trail forms numerous highways through Manitoba, Saskatchewan and Alberta and follows the approximate route that the Northwest Mounted Police (today's Royal Canadian Mounted Police) took on their march west in 1874. The goal of the Northwest Mounted Police was to shut down the Whisky Trade occurring at Western Canadian whisky forts and to establish law and order prior to the future settlement of Western Canada.

More specifically, this portion of Highway 4 is the approximate route that Commissioner James MacLeod took when he returned from Fort Benton with badly needed supplies and the indispensable guide, Jerry Potts. From Fort Benton, Jerry Potts then lead Commissioner MacLeod and his men to Fort Whoop-Up, and then on to establish Fort MacLeod, west of Lethbridge.



As the Red Coat Trail runs through the SEASP area, the Northwest Mounted Police and their history should be recognized in the plan area. This can include the naming of certain parks, streets or other landmarks after the Northwest Mounted Police and their march west. This can also include the establishment of a monument, gateway feature or interpretative board adjacent to Highway 4 at the entrance to the city in the far east of the plan area. This is further discussed in section 4.5.

#### 2.4.6 Objectives

- a) Ensure sites of historic importance and traditional use are protected and effectively incorporated into the land use concept.

#### 2.4.7 Policies

- a) The recommendations of the Traditional Resources Review (Section 2.4.2) shall be considered during the preparation of subsequent Outline Plans that include lands adjacent to Six-Mile Coulee. These areas are shown on Map 15 of this document as Outline Plans R1b and R3.
- b) If any historic resources are discovered during the development of the area, the Minister of Alberta Culture shall be notified and further instructions regarding the documentation of such resources, in compliance with the Historical Resources Act, shall be undertaken.
- c) It shall be encouraged for both the First Special Services Force and the Northwest Mounted Police to be recognized in the SEASP area. This recognition can include the

naming of certain parks, streets or other landmarks after the members of both groups and key historical milestones. This can also include the establishment of a monument, gateway feature or interpretative board adjacent to Highway 4 at the entrance to the city in the far east of the plan area.

- d) Land use in proximity to the Heritage Inventory Site (known as “Parry Farm”) shall be sensitive to the historic nature of the site, and shall maintain to the greatest extent possible the original character of the site, including the residence, yard and remaining tree grove.
- e) A park shall be established on a portion of the Heritage Inventory Site, known as “Parry Farm”. The type and size of park shall be further identified at the Outline Plan stage, with consideration being given to the remaining yard, tree grove and gardens on this site for utilization within this park.
- f) The landowner of the Heritage Inventory Site (known as “Parry Farm”), in partnership with the City of Lethbridge, is encouraged to pursue designation of the site as a Municipal Historic Resource. The City of Lethbridge will support an application to designate the site as such.

## 2.5 Safety

The safety of a community is enhanced through utilization of Crime Prevention Through Environmental Design (CPTED) principles in both the public and private realms. CPTED considers three key principles: Natural Surveillance, Natural Access Control and Territorial Reinforcement.

### CPTED Principles

**Natural Surveillance** includes measures to increase visibility and maintain sightlines, such as targeted lighting and the proper placement of hard (e.g., fences) and soft landscaping (e.g., plants).

**Natural Access Control** refers to measures that prevent or limit access to a site, and can include the use of hard and soft landscaping to inhibit access to a roof, or to deter unwanted graffiti.

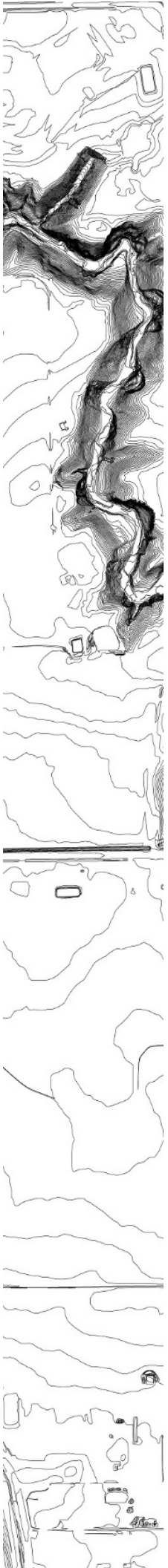
**Territorial Reinforcement** can be thought of as promoting a sense of ownership and responsibility for an area among those who live or work there. The thought being that people are more likely to report or prevent unwanted behavior in areas where they have pride and a vested interest.

Figure 4 CPTED Principles

Lethbridge Regional police and Fire & Emergency Services were consulted during the planning process and it was determined that no new police or fire stations would be required within the plan area in the future. Police service will continue to be based from the downtown station and fire service will be provided from the fire station that is nearest to the plan area.

At the Area Structure Plan stage, it is unknown if any areas in the SEASP area will be subject to the Alberta Building Code’s High Intensity Fire Requirements (HIRF – see Appendix D). This will be





better understood at the Outline Plan stage when more details in regards to specific land uses and local roadways are known. Emergency response time modeling will be performed at the Outline Plan stage and will be an input into the Outline Plans to determine the HIRF requirements. If HIRF requirements are no longer in place at the time of development, the preceding legislation will be used.

#### 2.5.1 Objectives

- a) Ensure that the SEASP area is designed with the safety of residents and property in mind.

#### 2.5.2 Policies

- a) CPTED principles will be considered in the environmental design and landscaping of the SEASP at the Outline Plan and development stages. These principles must be considered in both public and private realms, as both complement one another and must work together.

The City of Lethbridge shall complete emergency response time modeling at the Outline Plan stage to determine HIRF requirements. If HIRF requirements are no longer in place at the time of development, the preceding legislation will be used.

## 2.6 Planning & Policy Framework

### Planning & Policy Context

South Saskatchewan **Regional Plan**

City of Lethbridge and Lethbridge County **Intermunicipal Development Plan**

City of Lethbridge Integrated Community Sustainability Plan / **Municipal Development Plan**

Southeast Lethbridge **Urbanization Plan**

Transportation, Parks, Bikeways and Pathways, Recreation and Culture **Master Plans**

Land Use **Bylaw 5700 (LUB)**

Figure 5 Planning & Policy Context

#### 2.6.1 South Saskatchewan Regional Plan

On September 1, 2014, the SSRP became the second Regional Plan to be adopted in Alberta under authority of the *Alberta Land Stewardship Act (ALSA)*. Municipalities within the South



Saskatchewan region are required to ensure statutory compliance with the SSRP by August, 2019.

The SSRP is designed to achieve specific environmental, social and economic outcomes in the region using a cumulative effects approach. The province is tasked with monitoring compliance with the SSRP and evaluating outcomes using a series of Management Frameworks. Certain portions of the SSRP are meant to guide and inform land-use planners and decision makers, while others are legally binding for the provincial government, local governments and other decision makers. Key SSRP strategies that informed the SEASP include those listed on Table 1 - Select SSRP Strategies.

In accordance with Section 638.1 of the *Municipal Government Act* (MGA), in the event of a conflict or inconsistency between the SEASP and the SSRP, the SSRP shall prevail.



Figure 6 ALSA Regional Planning Areas

Table 1 Select SSRP Strategies

Select SSRP Strategies	
7.2	Explore and present potential new approaches to draw on the rich cultural, ecological, and traditional land-use knowledge and stewardship practices of aboriginal communities.
8.2	Address common planning issues, especially where valued natural features and historic resources are of interest to more than one stakeholder and where the possible effect of development transcends jurisdictional boundaries.
8.4	Anticipate, plan and set aside adequate land with the physical infrastructure and services required to accommodate future population growth and accompanying community development needs
8.7	Consider the value of intermunicipal development planning to address land use on fringe areas, airport vicinity protection plans or other areas of mutual interest.
8.12	Contribute to a healthy environment, a healthy economy and a high quality of life.
8.13	Provide a range of economic development opportunities, stimulate local employment growth and promote a healthy and stable economy.
8.14	Feature innovative housing designs, range of densities and housing types such as mixed-use, cluster developments, secondary suites, senior’s centres and affordable housing. Provide the opportunity for a variety of residential environments which feature innovative designs and densities and which make efficient use of existing facilities, infrastructure and public transportation.
8.16	Minimize potential conflict of land uses, within and adjacent, to areas prone to flooding, erosion, subsidence or wildfire.
8.18	Locate school and health facilities, transportation, transit and other amenities appropriately to meet increased demand from a growing population.
8.37	Identify the location, nature and purpose of key provincial transportation corridors and related facilities.
8.38	Work with the Ministry to minimize negative interactions between the transportation corridors and related facilities identified in accordance with Strategy 8.37; and the surrounding areas and land uses through the establishment of compatible land-use patterns.



2.6.2 Integrated Community Sustainability Plan / Municipal Development Plan

The Integrated Community Sustainability Plan / Municipal Development Plan (ICSP/MDP), adopted in 2010, directs future planning to promote local economic development and employment opportunities through the provision of sufficient, planned and serviced land. The ICSP/MDP also supports a range of choice for new residential, commercial and industrial expansion areas as well as multi-modal connectivity. The SEASP establishes a significant new area in Lethbridge for residential, commercial and industrial land use activities to take place.

The ICSP/MDP projects the continued expansion of residential, commercial and business industrial land uses for southeast Lethbridge, with residential and commercial south of 24th Avenue S / Highway 4 and industrial uses to the north. As the ICSP/MDP is a large overarching document providing a growth strategy for the entire City, its policies and concepts are general in nature and subject to further refinement at later planning stages, such as the SEASP.

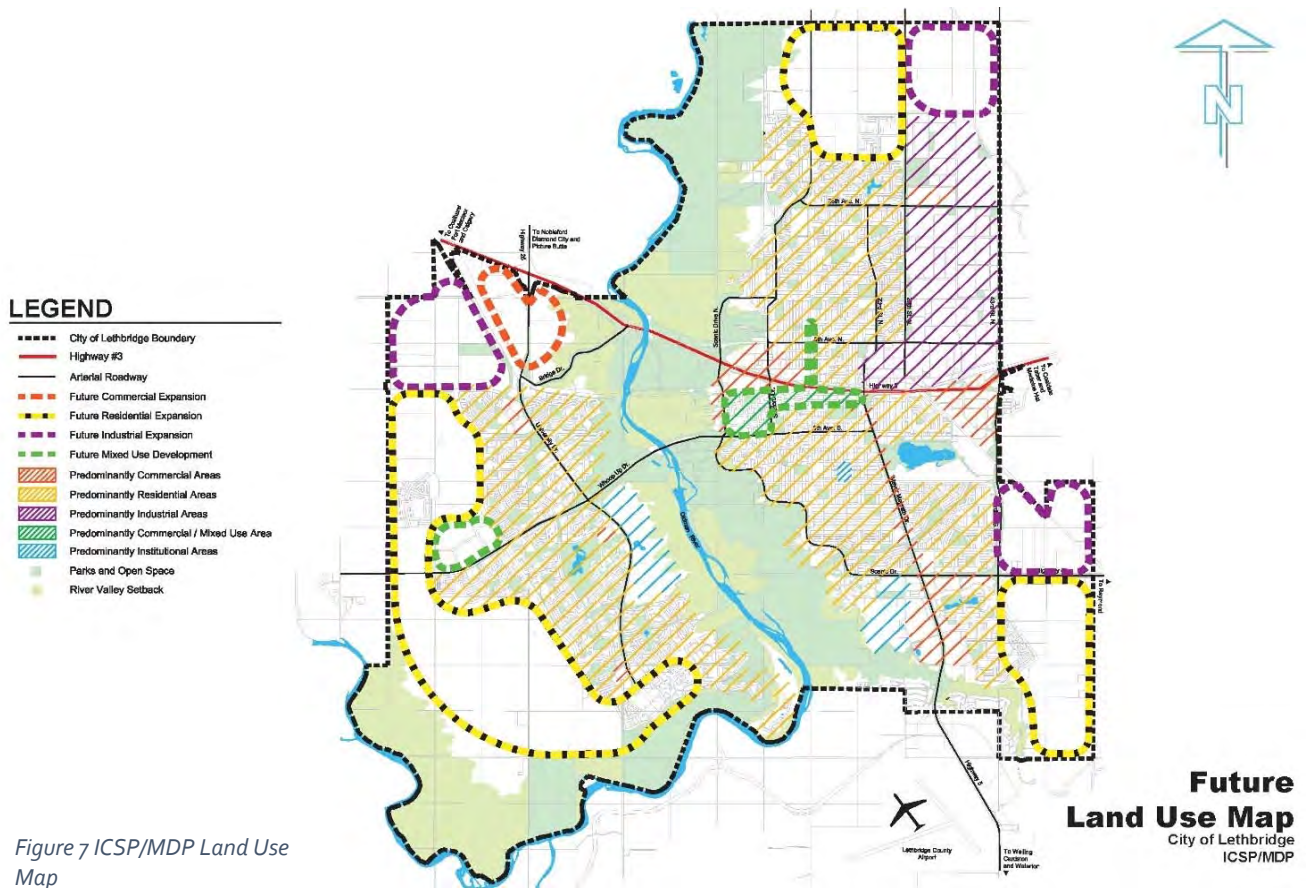


Figure 7 ICSP/MDP Land Use Map

The Outcomes and Policies of the ICSP/MDP, listed in Table 2 Select ICSP/MDP Outcomes, provided direction during the development of the SEASP.

Table 2 Select ICSP/MDP Outcomes

Select ICSP/MDP Outcomes	
6.1.1	Lethbridge is a Good Place to Open and Operate a Business
6.2.1	Lethbridge Has a Range of Housing that Meets Everyone’s Needs
6.4.1	Lethbridge is a Compact City
6.4.2	Lethbridge has an Efficient and Effective Integrated Transportation Network
6.4.3	Lethbridge is a Walkable, Bicycle Friendly City
6.4.4	Lethbridge is Expanding in a Responsible Manner
6.4.5	Lethbridge is a Planned City that Exhibits Quality Urban Design
6.5.1	Lethbridge’s River Valley is the Primary Open Space System
6.6.1	Lethbridge Has Strong Relationships with Neighbouring Communities

### 2.6.3 City of Lethbridge and County of Lethbridge Intermunicipal Development Plan

The City of Lethbridge and County of Lethbridge Intermunicipal Development Plan (IDP; 2004) is a statutory plan created under Section 631 of the MGA, and contains policies regarding the coordination of future development between the two municipalities.

The lands considered by the SEASP form part of Plan Area 3 in the IDP. As such, the City was required to circulate drafts of the ASP and communicate with the Lethbridge County during the development of the ASP.

Moreover, Section 5.4 of the IDP contains policies specific to the lands found within the SEASP area, particularly with regard to Six-Mile Coulee, the River Valley, City entrances and drainage. These policies reflect the importance the City and County give to Six-Mile Coulee and the River Valley with regard to environmental sensitivity, scenic and recreational attributes and water quality, as well as the importance of ensuring appropriate drainage and appealing entrances into the City along Highway 4.

The Policies highlighted in Table 3 Select IDP Policies are of particular relevance to the SEASP and were adhered to during its development.



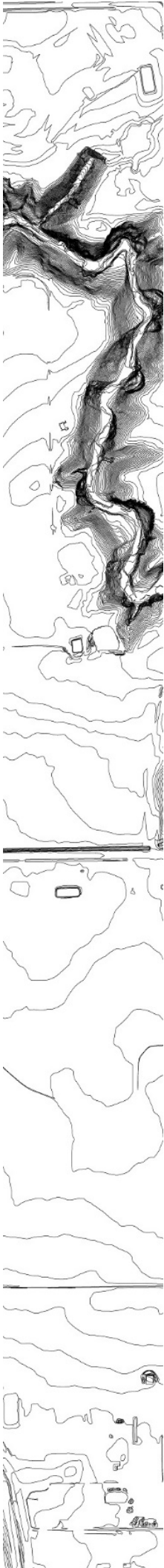


Table 3 Select IDP Policies

Select IDP Policies	
5.1.1	Each municipality will circulate for comment: With respect to the City, proposed area structure plans in Area 3 shown on Map 1 in Appendix 1.
5.3.3	Urban designs and City area structure plans should be prepared in such a way as to limit the number of entry points on roads that are either under Lethbridge County jurisdiction or link directly to the Lethbridge County road system.
5.4.3	When further information regarding water quality in the coulee is made available, both municipalities should request, as part of any application for subdivision or development, information regarding storm drainage.
5.4.5	With regard to Policy 5.1.19, areas serviced by rail and primary highways will be considered for uses other than agriculture.
5.4.8	All uses applied for in and near drainways that enter the river will be evaluated to ensure that water quality will be protected from contaminated runoff.
5.4.9	Development and subdivision applications should contain information regarding soil stability if the land is determined to be in a sensitive area.
5.4.13	The main City “entryways” shown as ... Highway 4 and 5 in the south, should be given special consideration by both municipalities for approvals to protect and enhance the view with special landscaping, signage or other features.

#### 2.6.4 Southeast Lethbridge Urbanization Plan

The Southeast Lethbridge Urbanization Plan (SELUP; 2003) provides a framework and strategy for the future development of lands in southeast Lethbridge. It is intended to provide a degree of certainty to both the public and City Council about the “form and character” of future urbanization areas, including:

- Major land patterns, major transportation corridors, preliminary infrastructure servicing and environmental protection for the area
- Community values and planning goals for directing and managing urbanization and change within the area
- Guidelines and a policy context for subsequent planning initiatives
- A framework within which adjacent landowners can work cooperatively
- An implementation strategy

The Urbanization Plan envisions the following defined areas in the southeast, and associated land uses:

- Gateway and regional commercial / employment centre along Highway 4
- Three villages, bound by Highway 4, Highway 5 (Mayor Magrath Drive S), Six-Mile Coulee and the City boundary (East, West and South Neighbourhoods)

- Commercial corridor along Highway 5
- Open space preservation and pathway network around Six-Mile Coulee

The areas and land use types envisioned by the SELUP continue to reflect good planning principles and the highest and best use of the land. According to the SELUP, the “employment centre” would comprise *“a range of business, office, institutional and light or clean industrial uses”*. It also states that, *“this use designation is based on the site’s excellent road and rail access. Conversely, the fragmentation of the land by rail and irrigation canal, the existence of some commercial uses and limited spatial extent of the area work against residential development in this area”*. The SEASP is consistent with this Urbanization Plan’s vision for the employment uses in this area.

In addition, the SEASP continues to promote Highways 4 and 5 as key commercial and employment areas, and important gateway corridors into the City. The area south of Highway 4 is also generally reserved for residential development in the SEASP, which is consistent with the Urbanization Plan. Six-Mile Coulee compliments these areas as an important natural feature on the landscape, and is integrated into the open space and pathway concept for the area.

Key departures of the SEASP from the Urbanization Plan include the relocation of 43 Street S as an arterial road to provide increased connectivity between new and existing residential areas. The SEASP also makes use of a modified grid road network which integrates residential areas rather than segregating them into separate villages. Through the additional background analysis conducted as part of the SEASP (and not typically conducted as part of a high-level plan like an Urbanization Plan) , it was discovered that areas north of the railway and in the far southeast corner of the plan area were not suitable for most forms of permanent development, due to a number of constraints. This is further explained in Sections 4.7 and 4.8 of this document.



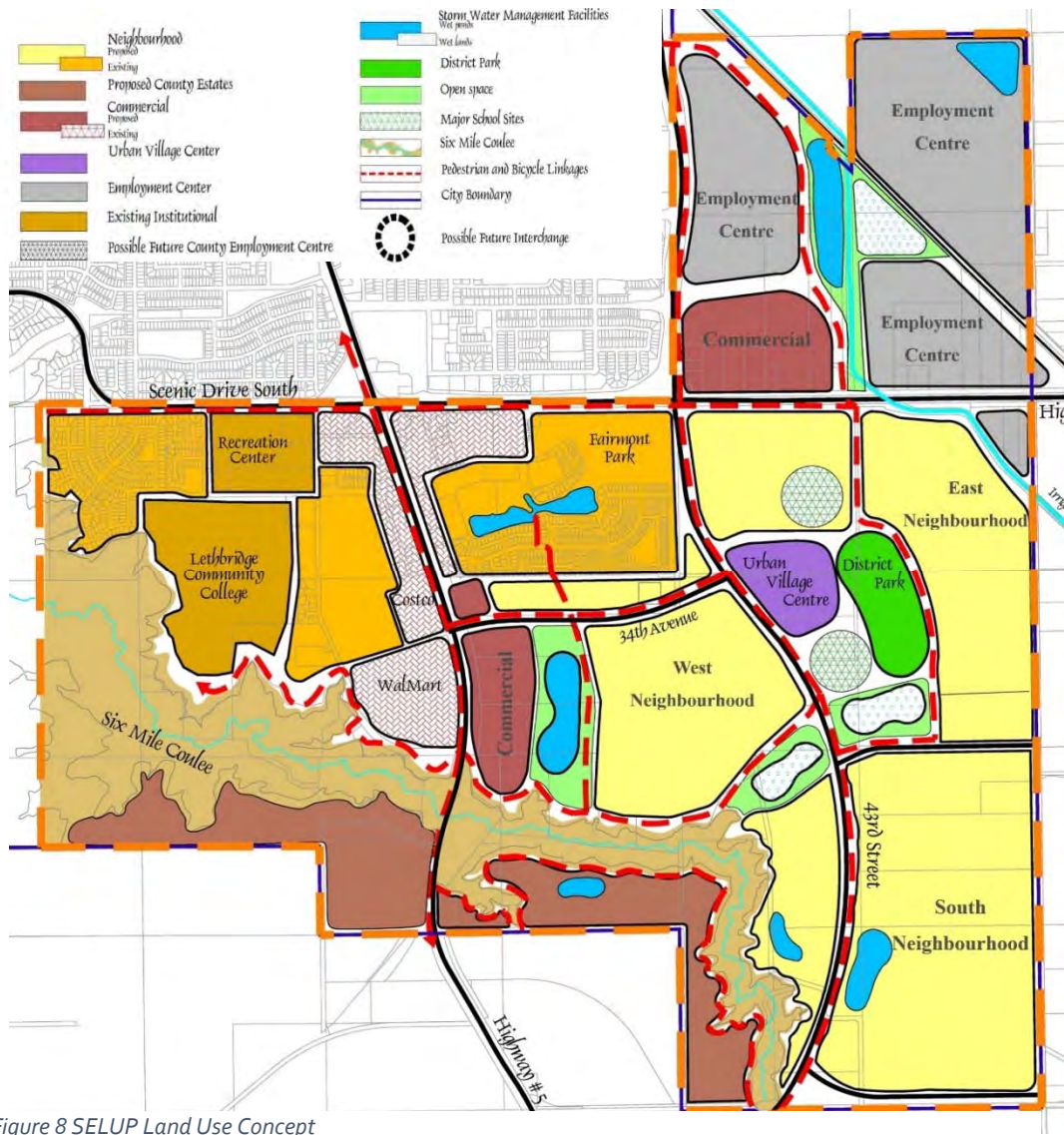


Figure 8 SELUP Land Use Concept

### 2.6.5 Transportation Master Plan

The Transportation Master Plan (TMP; 2013) envisions “an integrated multi-modal transportation system that will build upon roadway infrastructure to promote economic vitality and serve the requirements of the ICSP/MDP”. The TMP’s key objectives are to:

- Evaluate existing infrastructure to identify deficiencies;
- Identify transportation infrastructure requirements;
- Identify future upgrades;
- Identify goals to encourage multi-modal transportation;
- Engage the community; and,

- Develop an action plan.

The TMP identifies phased transportation improvements through 2040 based on projected populations. For the southeast portion of the City, anticipated works / upgrades include:

- Expansion of gateway corridor along Mayor Magrath Drive S / Highway 5
- Upgrade to the Scenic Drive S / Highway 4 and Mayor Magrath Drive S / Highway 5 intersection

These works will enhance the connectivity between southeast Lethbridge and other residential, commercial and industrial areas of Lethbridge, as well as important transportation routes connecting to areas outside the City (mainly Highways 4 and 5).

#### 2.6.6 Parks Master Plan

The Parks Master Plan (PMP; 2007) is a “comprehensive document designed to assist the City in facilitating a strategic expansion and retooling of the parks system.” The PMP provides key advice to new growth areas by informing the preparation of Area Structure Plans.

Section 8.2.2 of the PMP recommends that the City continue to acquire dedicated parklands in new growth areas and that parklands incorporate natural areas and stormwater management facilities. The PMP also encourages the creation of special use parks (e.g., mountain bike parks, off leash dog parks, water spray parks) and preservation areas where lands contain ravines, coulees, high concentrations of wildlife habitat and heritage sites.

#### 2.6.7 Bikeways and Pathways Master Plan

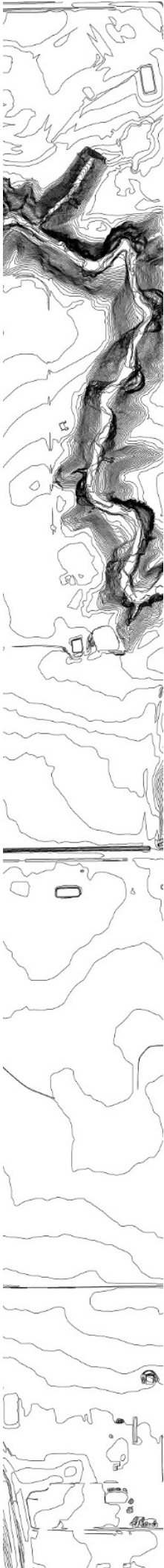
The Bikeways and Pathways Master Plan (BPMP; 2007) “provides a comprehensive framework to develop and / or enhance the city’s multi-use trail network, identifies key connections and provides the tools to develop and promote active living through increased pathway use through an integrated approach to planning and delivery.” Through public consultation, a series of short, mid and long term priorities are identified and form the BPMP list of recommendations.

The BPMP identifies Regional Multi-use Pathways and High Speed Regional Commuter Separated Pathways for areas adjoining the SEASP area. The SEASP expands upon this recommendation and outlines interface points between the internal bikeways and pathways network and the existing network in established neighbourhoods to the west of the plan area.

#### 2.6.8 Recreation and Culture Master Plan

The Recreation and Culture Master Plan (RCMP, 2013) is intended to provide “an accurate depiction of the present and future needs for recreation and culture facilities and services in the City of Lethbridge and community, and outline strategies as to how to meet identified needs for a ten year period.”





The Recreation and Culture Master Plan outlines short, mid and long-term priorities for new facilities and reinvestments into existing facilities in the next 10 years. Given that the SEASP area is largely greenfield, there are no existing facilities. During the preparation of the SEASP, the Community Services Department was consulted to facilitate the achievement of Key Recreation and Culture Master Plan Objectives by locating new facilities in the plan area. These objectives have been incorporated into this Plan.

#### 2.6.9 Land Use Bylaw 5700

Most of the lands within the plan area are designated under Land Use Bylaw 5700 (LUB; 2011) as being in the Future Urban Development (FUD) district. The purpose of designating land in this district is “for the control of subdivision and development until the required municipal services are available, area structure or area redevelopment plans are approved, and more appropriate alternative districts are applied.”

Various other parcels of land have also been designated as Direct Control districts which have individual site specific purposes that “require specific sets of rules in order to achieve a desired result”. Within this area, the Direct Control designation has been employed to allow the landowners to develop their properties with uses that were not dependent on municipal servicing or a great deal of investment - recognizing that the SEASP would eventually be created and that future urban development would occur.

#### 2.6.10 Objectives

- a) Ensure that the SEASP is drafted in accordance with all superseding plans and bylaws.
- b) Allow for constructive and open communication between the City, Lethbridge County and adjacent landowners in the City and County.

#### 2.6.11 Policies

- a) The SEASP shall be developed in compliance with the following statutory plans: the SSRP, the ICSP/MDP and the IDP.
- b) The Southeast Area Structure Plan shall have consideration for other statutory and non-statutory documents that are relevant to the plan area, including those discussed in Sections 2.6.1 through 2.6.9.
- c) As per the City of Lethbridge and Lethbridge County Intermunicipal Development Plan, Lethbridge County shall be circulated notification with regard to any amendment to the SEASP.
- d) Lethbridge County shall be circulated notification for any Land Use Bylaw amendments or for any discretionary development permits that are adjacent to the shared municipal boundary.
- e) The TMP shall be amended to reflect the road network outlined in the SEASP.



# 3.0 Development Potential

## 3.1 Market Analysis

Technical background to the SEASP a Market Analysis was completed in 2014. The Analysis projects demand for business industrial, office commercial, retail commercial and residential land uses for the plan area beyond those land uses previously identified in already-approved Area Structure Plans and Outline Plans.

The purpose of the Market Analysis and the accompanying forecasts is to estimate the market demand and potential for a variety of land uses in southeast Lethbridge as accurately as possible. Moreover, the Market Analysis considers the timing of projected demand (build-out). Together, this information informs the creation of policies that promote realistic and sustainable land use development as required through the ICSP/MDP.

## 3.2 Study Area

The Analysis study area is approximately 700 ha in size and corresponds to the SEASP area. The Analysis focuses primarily on conditions within the plan area, however, as the plan area will additionally serve a regional function—drawing consumers and workers from other parts of the City and region—other areas must also be considered to formulate results.

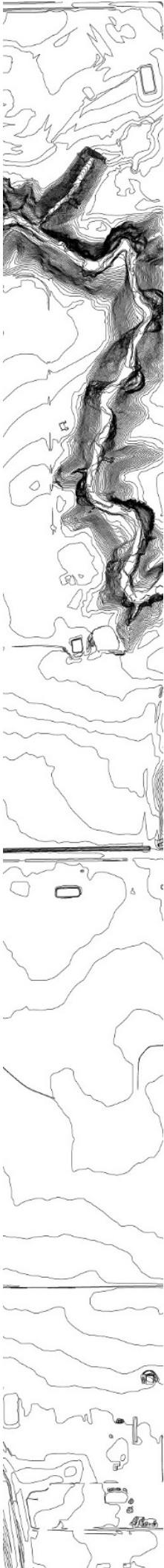
The following other geographies were also considered:

**South Lethbridge.** At the time the Market Analysis was completed, south Lethbridge had just over 30,000 residents and contained the majority of retail space in Lethbridge. As a result, many residents and visitors travel to this area of the City for their shopping needs. South Lethbridge has also historically been seen as a preferred residential and commercial area. While people’s perceptions of the City are based on their own experiences, they have implications in the real world and in the market place. Bringing additional residential and commercial lands on-line helps meet the market demand among those looking to reside or open a business in south Lethbridge.

**City of Lethbridge.** The SEASP area will contain a number of uses that will draw residents and consumers from across the City. As such, it will influence the marketplace in other sectors of Lethbridge.

**Regional Trade Area.** Many land uses in the plan area are likely to serve the regional market. This is especially true for municipalities to the east and south of Lethbridge, such as Coaldale, Raymond and Cardston. These areas have excellent access to transportation corridors (e.g., Highways 4 and 5). For the purpose of this Analysis, Lethbridge’s regional trade area extends to the eastern boundaries of the Municipal District of Taber and the County of Warner No.5 in the east, the United States border to the south, the easternmost





portion of the Regional District of East Kootenay, B.C. (including Sparwood and Fernie), and southern portions of the Municipal District of Willow Creek and Vulcan County to the north. This regional trade area contains approximately 200,000 residents.

A secondary trade area extending further to the west (trade area of Cranbrook), the east (trade area of Medicine Hat) and the south (into the USA) is not considered by this Market Analysis, as a comparatively small amount of business comes from the secondary trade area and its impact is difficult to quantify.

### 3.3 Market Demand

The Market Analysis uses a 50-year time frame and historical growth trends to project future market demands within the plan area for industrial, commercial and residential land uses.

The following table summarizes the results of the Analysis, and shows the market demand for business industrial, retail commercial and residential land over the next 50 years. The summary provides a moderate estimate of market demands that are the result of various market calculations (see the full Market study in the Technical Documents Appendix for more information on methodology).

Table 4 Summary of SEASP Land Area, Floor Area and Population Forecasts through 2063

Development Type	Land Area (net ha)	Population	Floor Area (Square Feet)
Residential	220	16,500	N/A
Business Industrial	21	N/A	300,000
Office Commercial	31	N/A	850,000
Retail Commercial	15	N/A	400,000

Table 4 suggests an ideal mix of land uses. The Market Analysis does not consider the limiting effects of topography, limitations for servicing infrastructure nor the potential for land value

fluctuations based on more subjective considerations (e.g., specific lands within the plan area are likely to be more appealing for certain uses, such as lands adjacent to Highway 4 which are likely to attract commercial development). The result of this analysis manifests as a land use concept, presented in Section 4 of this ASP and which will be further refined through subsequent Outline Plans.

Based upon the Analysis, the largest land use will be for residential development, with demand for commercial and business industrial development as well. Where appropriate, these land uses are represented in the SEASP land use concept.

Based on the projected land uses in the SEASP area and overall City-wide market, it is estimated that the entire SEASP will reach full development over approximately 50 years.

# 4.0 Land Use Concept

## 4.1 General Overview

The land use concept for the Southeast Area Structure Plan is informed by a number of plans and statutory documents, including the SSRP, the City's ICSP/MDP, land use plans for adjoining neighbourhoods and the Corporate Master Plans of various City Departments. In particular, the SEASP Land Use Concept is driven by the vision and policies of the ICSP/MDP that support well-designed, connected and livable neighbourhoods through the incorporation of multi-modal transportation options, community gathering places, land stewardship best practices and a mix of housing types.

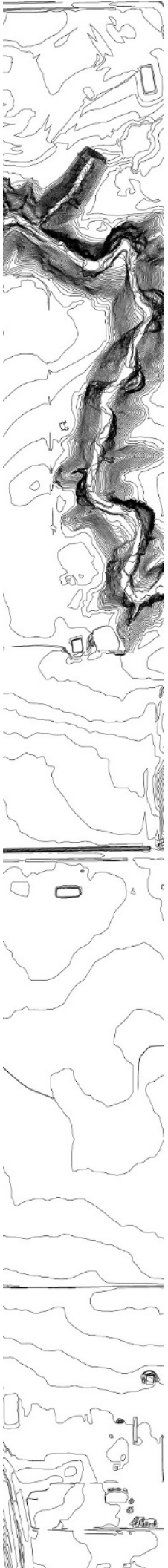
Map 7 illustrates the land use concept for the SEASP. The coloured areas and symbols on the Land Use Concept map demonstrate the vision for the future land use of the area which will be home to approximately 16,000 residents and a number of businesses and public uses. The land use concept is intended to be highly conceptual in nature and shows a general relationship between the various land uses. The land use concept is also generally consistent with higher level plans that have previously been completed. Subsequent Outline Plans will further refine this land use concept.

The SEASP area is intended to be an area where people can live, work and play. The City's ICSP/MDP and the South East Urbanization Plan envisioned the lands north of 24 Avenue S for commercial and business industrial type uses that create employment for existing and future residents on the City's south side. Through the public engagement conducted for the SEASP it became apparent that this vision for the lands north of 24 Avenue S was not universally shared by the landowners and in particular all of those west of the SMRID irrigation canal. As a result, this area is further discussed in greater detail under the Special Planning Area section of this document.

Lands south of 24 Avenue S are best suited for primarily residential use as they will be integrated into existing neighbourhoods to the west through strong transportation connections, the pathway system and utility infrastructure. In order to create a well-functioning and complete neighbourhood, commercial and public uses that provide services and support to residents are also appropriate to be located here.

A strong network of internal connections is also required, primarily between residential and non-residential uses. This set of connections will similarly come in the form of pathways and roads (see Sections 5 and 6) utilizing open space, as well as through the clustering of compatible uses in proximity to one-another.





## 4.2 Residential

The SEASP area will provide a choice in housing options and densities for future residents, including single-detached and medium density multi-dwelling unit options. This will assist in achieving the ICSP/MDP goal of providing a range of housing that meets everyone's needs.

The majority of dwelling units in the plan area will be single detached dwellings, as this is the most popular housing choice in the City and will likely continue to be in the future. However, this plan estimates that approximately 40% of dwelling units in the SEASP area have the potential to be multi-unit dwellings. Within the area defined as Residential in this ASP, Multi-unit housing can comprise a range of dwelling types including townhouses and apartments and represent a significant share of total dwellings in this area. This is consistent with figures in other ASPs, elsewhere in the city, that have recently been approved and is greater than the share of multi-unit dwellings comprising approximately 33% of all dwellings located in existing neighbourhoods<sup>1</sup>

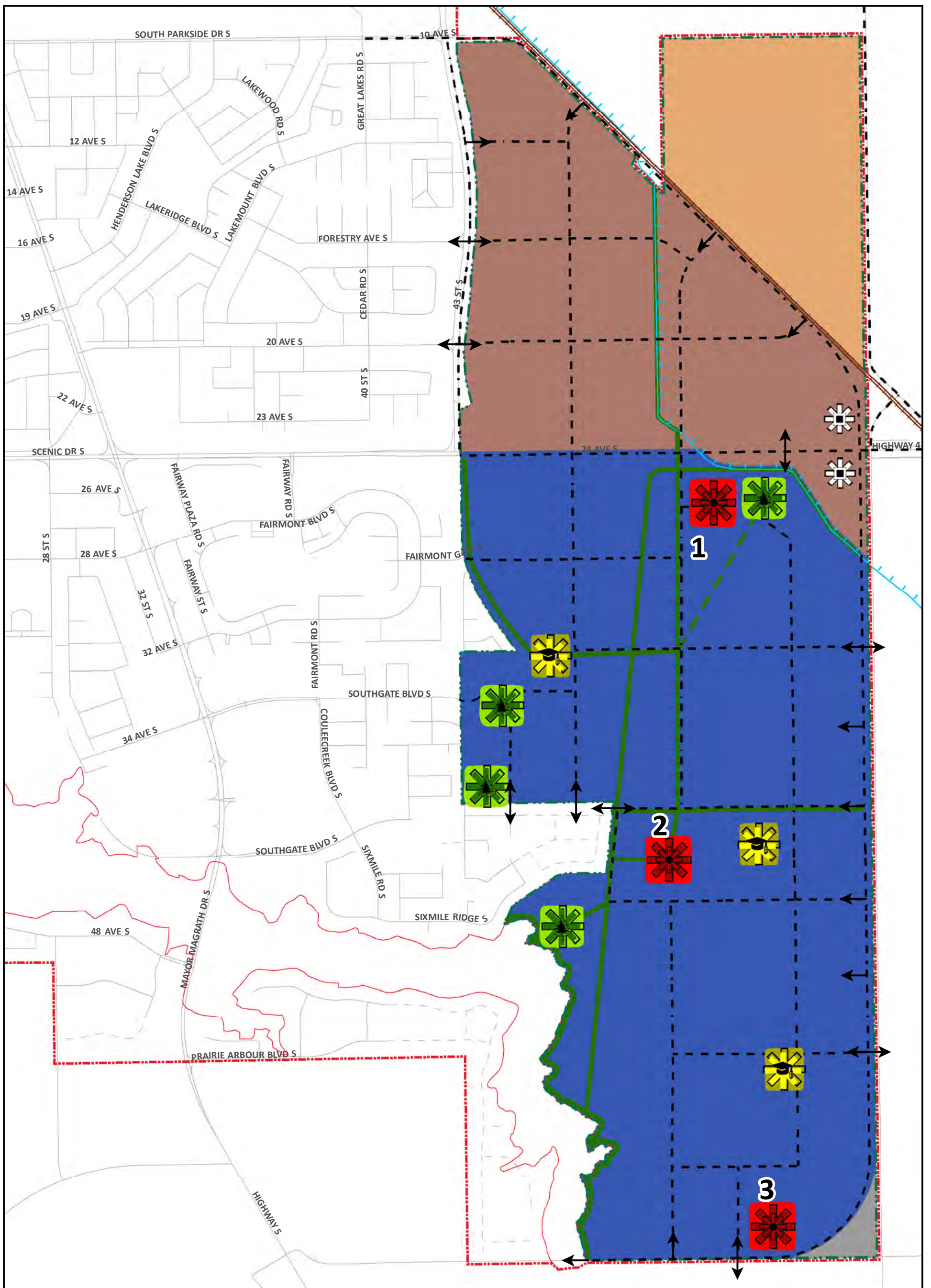
The availability of secondary suites is also an important consideration within the plan area, as secondary suites can provide affordable accommodation to tenants and provide homeowners with additional income to assist with household finances. They can also house elderly parents and relatives that need supportive care. They are also an important housing option considering the City's large post-secondary student population that lives off campus. Secondary suites promote densification and the efficient use of land in low density residential areas, however, care must also be taken to ensure that they are both safe and legally constructed. Secondary Suites must undergo review and inspection in order to obtain the required development and building permits.



*"Aayyyy! Who lives in a secondary suite? This Guy. They are the Coolest!"*  
CC Image courtesy of Richard Elzey on Flickr

The current Land Use Bylaw #5700 allows secondary suites as either permitted or discretionary uses in select land use districts. This LUB also provides regulations for approved secondary suites such as requiring a minimum of one additional parking stall per secondary suites and requiring separate access from the principle dwelling. To allow and encourage the market to construct this type of development, a target of 30% of low density residential parcels having secondary suites is desired in the SEASP area. Specific guidelines or locations that dictate the potential location of secondary suites will be provided at the Outline Plan stage and Outline Plans will be drafted with the provision of secondary suites in mind.

<sup>1</sup> Statistics Canada. 2012. Lethbridge, Alberta (Code 4802012) and Division No. 2, Alberta (Code 4802) (table). Census Profile. 2011 Census. Statistics Canada Catalogue no. 98-316-XWE. Ottawa. Released October 24, 2012. <http://www12.statcan.gc.ca/census-recensement/2011/dp-pd/prof/index.cfm?Lang=E> (accessed May 1, 2015).



**Legend**

-  Southeast Area Structure Plan
-  City Boundary
-  Railway
-  SMRID Irrigation Canal
-  Safe Development Setback Line
-  Existing Roads
-  Planned External Roads
-  Proposed Roads
-  Potential Connection
-  Park Pathway
-  Special Planning Area
-  Residential
-  Semi Rural
-  Undevelopable
-  Community Node
-  Major Park
-  School
-  Gateway Feature
-  Roadway Connection

# MAP 7 - Land Use Concept

The Southeast plan area will contain a number of land uses that offer options to live, work, and play within the area. A variety of community, neighbourhood, and park nodes are located throughout the plan area to facilitate and encourage citizen interaction.





Lethbridge has the highest rate of child poverty (one in five children in Lethbridge are impacted by poverty) and the second highest low-income rate in the province<sup>2</sup>. Poverty impacts all aspects of our community and makes housing that is safe and affordable inaccessible for a number of the City's residents. As the SEASP area will account for a substantial amount of residential growth in the City, the provision of affordable housing in this area is important for the City as a whole. To create or encourage the construction of affordable housing units in the plan area a number of financial and policy instruments, that range from regulatory to incentive-based, can be utilized.

An example of an incentive-based tool for achieving affordable housing is a tax abatement program or subsidy for developers who choose to develop affordable housing. Another example is the relaxation of certain zoning regulations, such as maximum density or minimum required setbacks for developments that include affordable housing.

At this time municipalities in Alberta do not have as much experience with the legislation of affordable housing. However, in the future, it is possible that municipalities may have an increased ability to mandate affordable housing in new developments. Changes in this area are dependent on what changes are implemented to the *Municipal Government Act (MGA)*, which is currently under review. The updated MGA is anticipated to be approved in 2016 and it is intended that the SEASP be compatible with this legislation, especially with regard to the provision of affordable housing. As such, future Outline Plans developed in the SEASP area shall demonstrate that the provision of affordable housing through various financial or policy instruments has been considered in compliance with current provincial legislation.

Medium density residential uses consist of multi-unit dwellings up to 75 units per net hectare, and will be located in proximity to community gathering places, public and institutional sites, higher capacity roadways and commercial areas to take advantage of the opportunities presented by these other uses, and to facilitate multi-modal transportation options. The location of these medium density residential sites is not specifically shown in this ASP. Due to the size and long term build-out of the SEASP area, the location of medium density sites is better addressed at the Outline Plan stage.

Residential areas will also be enhanced through the strategic incorporation of open spaces and pathways in support of connectivity and strong environmental stewardship (see Section 5).

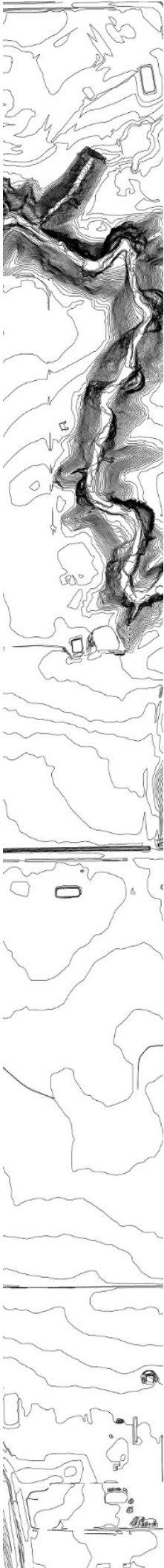
#### 4.2.1 Objectives

- a) Ensure the SEASP area contains a range of housing that meets the needs of everyone.
- b) Allow flexibility for new, future forms of housing that have not yet been implemented in Lethbridge.

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<sup>2</sup> Vibrant Lethbridge .*Low Income in Lethbridge: A Profile*. Lethbridge: City of Lethbridge Community and Social Development, 2015.



- 
- c) Allow for the combination of residential land uses with other appropriate land uses.

#### 4.2.2 Policies

- a) The majority of dwelling units in the SEASP area will be low density residential dwellings (i.e. single detached dwellings and duplexes). However, this area shall also contain a considerable amount of multi-unit dwellings (i.e. townhouses, low-rise apartments), similar to other neighbourhoods in the City.
- b) The general categories of the land uses that were identified under Section 4.2.2 Policy a), shall be refined further through the Land Use Bylaw districts that are applied to the Residential area. Appropriate Land Use Bylaw districts for this area are identified in Appendix B.
- c) Developments in the plan area that contribute to an increase in affordable housing units and the use of financial and policy instruments to achieve this increase shall be supported.
- d) Future Outline Plans developed in the SEASP area shall be compliant with provincial legislation as it relates to the provision of affordable housing.
- e) Outline Plans will allow and encourage the development of secondary suites. A target of 30% of low density residential parcels allowing secondary suites as a permitted or a discretionary use is encouraged. If the City's ICSP/MDP include targets greater than this figure in the future, Outline Plans must adhere to these greater targets.
- f) Secondary suites shall meet all municipal bylaws and necessary building and fire codes and shall obtain the required development and building permits to be considered a legal use.
- g) In addition to any safety or building requirements, Secondary Suites shall adhere to the requirements from the Land Use Bylaw.

### 4.3 Special Planning Area

Within the SEASP area, the lands north of 24 Avenue S are unique in many ways. There are a variety of existing agricultural, commercial, recreational, residential and religious assembly land uses such as a golf course, an electric substation, an RV dealership, a nursery, and storage yards and private workshops. These existing land uses are for the most part concentrated near major road ways and some require large tracts of land. Landowners in some cases have obtained the ability to develop their properties on an interim basis through a Direct Control district prior to an Area Structure Plan being adopted by City Council or before the provision of all municipal services.

In addition to these existing uses, there are also a number of existing physical constraints located in the Special Planning Area that need to be considered, such as the SMRID canal, a high pressure gas pipeline and a rail line. The land has also been subdivided in the past into a



number of smaller parcels and as a result there are the opinions and desires of 21 groups of landowners to consider. Before any development occurs in the Special Planning Area, astute attention must be given to these existing conditions through this ASP and any future planning exercises, as many of these conditions will remain into the future.

In an effort to follow approved City plans, incorporate professional planning knowledge, allow some flexibility and consider the opinions and desires of landowners, the Special Planning Area was created. Two potential land use options for the future development of this area have arisen from the planning process of this ASP and are discussed below. It is possible that when the time comes for an Outline Plan to be prepared by the landowners and their consultants, another hybrid option may also present itself.

### Option A

Option A is the development option that is preferred by City Administration, as it will provide an area of significant employment that will serve both the existing population and the future population growth of South Lethbridge. City Administration also noted that the severance of the Special Planning Area from the rest of the community, by the location of 24 Avenue S, 43 Street S and the railway inhibits the ability for community services such as transit, education and recreation to be provided in an efficient or effective manner.

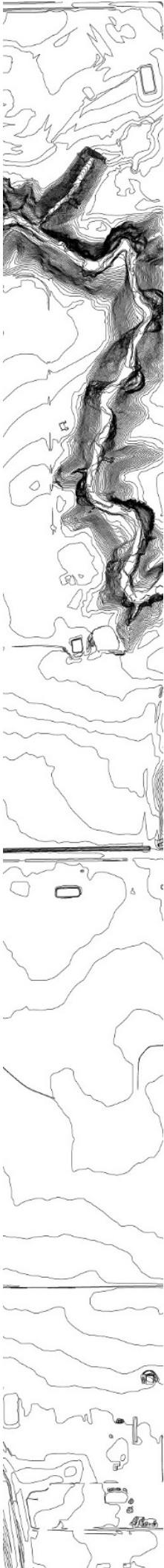
The existing planning policy for this area, including the Integrated Community Sustainability Plan/Municipal Development Plan and the South East Lethbridge Urbanization Plan, recognize the unique locational opportunities presented by this area. The Special Planning Area has key transportation linkages to Highways 3 and 4, which form a portion of the CANAMEX Trade corridor - which is Alberta's main trade route with the United States and Mexico.

Land uses which would benefit from being located near these transportation linkages would include businesses that benefit from exposure to high volumes of traffic or offer services to both the travelling public and employees within the transportation industry. The existing planning policy for the City also recognizes that this triangle of land could support additional employment based businesses (such as offices) that would benefit from the transportation linkages both within and outside of the City and allow employees a relatively easy commute to work either by transit, cycling, walking or private vehicle. Secondary uses that would complement those listed above, or that would require larger tracts of land that could be supplied in this area, could include retail, restaurants, religious assemblies, recreation, public utilities and forms

**Definition**

Business Industrial land uses do not adversely impact the surrounding area as they produce few off-site impacts, such as noise and emissions. A number of uses that are found in the business industrial areas could also be found in areas that are more commercial in nature. The W.T. Hill Business Park is a local example that contains many business industrial uses. Business industrial uses can include offices, business support services such as building security services, engineering or architecture firms; vehicle sales and rentals, storage and specialty manufacturing such as bakeries and furniture makers.





of medium or high density residential development such as housing for senior citizens or those employed in nearby businesses. In order to develop these type of uses, the lands could be designated as a commercial, business industrial or direct control land use district as described in the City of Lethbridge's Land Use Bylaw.

The Special Planning Area is adjacent to the rail line from the United States, however, due to changing requirements of the rail industry, there is currently not enough track length within the City boundary to construct a rail yard or rail spurs that would accommodate more than a handful of rail cars. A newly constructed, privately operated multi-modal rail facility adjacent to Highway 4 in Lethbridge County, is located 12 kilometers to the south of the SEASP area. Another rail yard is also located a few kilometers to the northwest of the City at Kipp. Additionally, the City of Lethbridge operates a small transloading rail facility in the northeast part of the City. These rail facilities are well positioned to handle the rail shipping needs of any business uses in the SEASP area that require access to rail shipping services.

The Special Planning Area will be buffered from residential neighbourhoods on the west and south by 43 Street S and 24 Avenue S (Highway 4). However additional transitioning measures must also be undertaken in the development of this area. Such measures can include the use of increased perimeter landscaping along the adjacent arterial roadways and locating business industrial uses towards the interior and commercial uses towards the perimeter of the Special Planning Area. This will be a requirement to be addressed in greater detail at the Outline Plan stage.

General manufacturing or heavy industrial type businesses such as food processing were not seen as appropriate for this area due to the potential for off-site impacts and the availability of appropriately zoned and serviced industrial lands in north Lethbridge. As well, low density residential development, including single detached dwellings, were also not seen as appropriate due to the physically isolated nature of the triangle of land and the lack of neighbourhood connection, community services and associated amenities homeowners have come to expect in the City. As described in the Market Study (see section 3 and the Technical Documents Appendix), there are many other areas within Lethbridge (including, but not limited to the portion of the SEASP area south of Highway 4) where low density residential



*Examples of business industrial (left) and heavy industrial development (right). Business industrial land uses are seen as appropriate under Option A for the Special Planning Area, while general or heavy industrial land uses are not.*

development has been planned and is more suitable from an economic, infrastructure servicing, neighbourhood design and community benefit perspective. In addition well designed, accessible and connected neighbourhoods have the potential to develop faster than this area beyond what would otherwise occur if the area was developed for employment-generating uses instead.

City Administration's analysis suggests the highest and best use of this triangle of land north of 24 Avenue S is for employment generating land uses. This is also consistent with previous planning documentation that has been produced since this area was brought into the City through the 1984 annexation, such as the City's exiting and previous Municipal Development Plans, the Southeast Lethbridge Urbanization Study and the Annexation Study.

### Option B

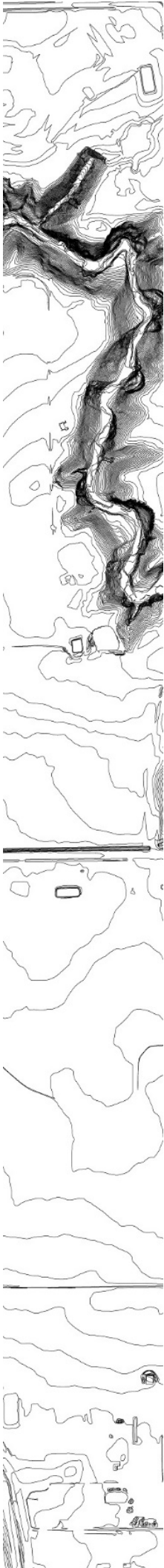
Through the public engagement for the SEASP it has become apparent that all of the landowners west of the SMRID canal and a few landowners east of the canal would prefer residential development (a mix of low, medium and high density) as the primary land use found in the Special Planning Area with significant neighbourhood commercial and public/institutional uses envisioned (see comments in the Technical Documents Appendix). The opportunity to locate larger scale commercial and employment uses at the far north end at 10 Avenue S and in a strip along 24 Avenue S was also supported by these landowners (see Figure 9).

These landowners believe residential use is more complimentary and a better fit with the existing land uses and will enhance the plans each landowner has for their own land. Residential and employment uses can easily co-exist when there is a connection or linkage between them. For example the building of a new religious assembly and associated seniors housing, would be enhanced by surrounding residential use as seniors would feel more "at home". At the same time smaller scale neighbourhood commercial business opportunities that serve the needs of the seniors population would be drawn to co-locate in the area. Business industrial was not desired due to the potentially unsightly nature and off site impacts of some uses and how that would impact existing residences and future development and potential residents.

Landowners also believe that based on what has been successful in the past in the Lethbridge market, a variety of housing options and densities ranging from single detached dwellings to higher density multi-storey buildings would be quicker to develop and allow landowners to see their visions and plans realized faster. Some of these landowners have owned their properties for a number of years; some even prior to the 1984 annexation into the City from the County. They have waited decades for development of their lands to actually become possible.

The landowners believe that the development of a residential neighbourhood (low, medium and high density) with public/institutional use and neighbourhood commercial use will result in a better end product for the City. The location of larger scale commercial and employment uses along 24<sup>th</sup> Avenue and at the north end of the Special Planning Area is a more realistic allocation based on the current commercial vacancies in the downtown core and the slow





absorption of other commercial and employment lands. In addition, residential uses are seen as more acceptable for the existing residents west of 43 Street S that for years have had agricultural land as their neighbour.

In regards to the broader interests of the City as a whole, these landowners have suggested that the development of significant sized employment areas within southeast Lethbridge should not be a priority, as it is not in the best interests of the City of Lethbridge. They believe that large-scale employment development in southeast Lethbridge will increase traffic congestion on the two existing river crossings from west to east Lethbridge and will increase the need for a third river crossing. The City of Lethbridge should place very strong emphasis on developing employment and further shopping developments in West Lethbridge and that this will reduce the traffic congestion on the two existing river crossings and delay the need for a third river crossing.

In the opinion of these landowners the proposed Option B in the Special Planning Area will proceed much more rapidly than the proposed employment/light industrial development shown in Option A. Residential development, proposed by this ASP, south of 24 Avenue S (Highway 4) requires sanitary services to cross through the Special Planning Area. These landowners feel that more rapid development in the Special Planning Area will also complement and enhance the ability to provide services for residential development south of 24 Avenue S. They note that the W.T. Hill Business Park, a business/light industrial development, commenced 18 years ago and to date is approximately half developed.

Development of the Special Planning Area as proposed in Option B provides for residential development with sufficient area to meet the long term needs for business/light industrial development in the southeast of the City.

An illustration of the land uses that are described in Option B within the Special Planning Area has been provided by these landowners in Figure 9 below.

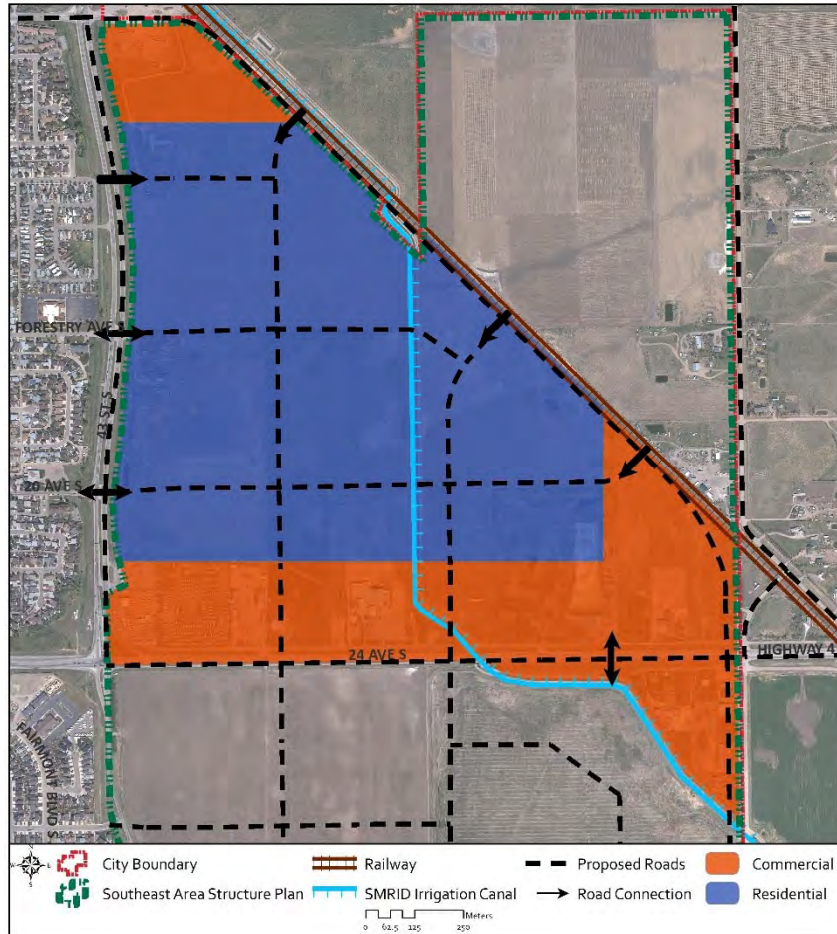


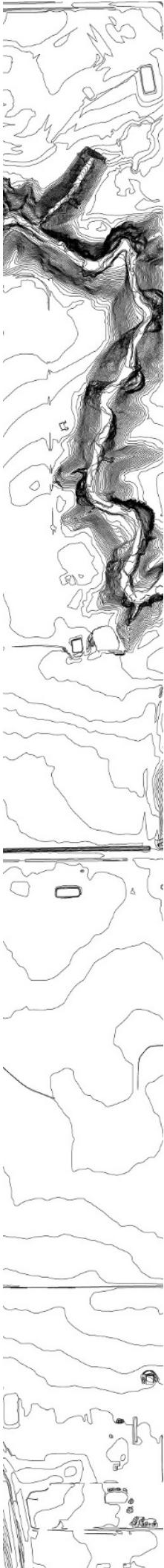
Figure 9 Special Planning Area Option B Land Use Map

### Option A or Option B; compliance with the ICSP/MDP

The land uses and vision articulated for Option A are in compliance with the current ICSP/MDP which was approved in 2010. The land uses and vision articulated for Option B are a departure from the stated vision for the lands in the current ICSP/MDP as shown in Figure 10. With the passing of the South Saskatchewan Regional Plan the City has embarked on a 4 year data gathering and target setting project which will culminate in the review of the ICSP/MDP in 2019. City Council as the approval authority for the new ICSP/MDP will have the opportunity to review the contents of the new document including the future land uses as shown in Figure 10. City Council has the ability to allow for Option A, Option B or a hybrid to be considered rather than limiting the Special Planning Area to only predominately industrial or employment uses as the current ICSP/MDP states.

The approval of the SE ASP as it is written, prior to the review of the ICSP/MDP is not premature as regardless of the direction City Council may take with the review of future land uses for the Special Planning Area, differing Options have been clearly laid out to cover any conceived change or confirmation in policy. It is not anticipated that the SEASP will need to be amended as a result. If City Council wishes to make a decision in regards to the vision for the Special Planning Area at this point in time rather than leaving in both Options, it is





important to remember that only Option A complies with the current ICSP/MDP. If only Option B is chosen an amendment to the ICSP/MDP is needed first before adoption of the ASP.

### Option A or Option B; initiating an Outline Plan

Landowners/developers usually initiate an Outline Plan based on the availability of infrastructure needed to service their lands and growth occurring adjacent to them (to avoid “skip development”). There is a substantial cost to preparing an Outline Plan both in terms of time and the planning and engineering expertise to write the document. Due to this substantial cost it is advisable that the vision and land uses for the Special Planning Area be reviewed through the update to the ICSP/MDP in 2019 before any work on an Outline Plan is initiated by landowners/developers.

The cost associated with providing infrastructure to the Special Planning Area is also a reason to delay the initiation of an Outline Plan. The City has indicated (through the off-site levy funding) that based on the rate of growth and infrastructure projects currently allocated funding, servicing the Special Planning Area is approximately 10-15 years away. The preparation of an Outline Plan so far in advance of development actually occurring could lead to unintended consequences for the landowners/developers and does not seem prudent.

### Option A or Option B; points to be addressed when preparing an Outline Plan

The actual physical transitioning between new and existing uses occurs through the Outline Plan, which is a detailed, finer grained planning exercise than an Area Structure Plan. The Outline Plan level of detail includes phasing strategies for the provision of servicing in tandem with construction of roads and lots, engineering calculations for water, sanitary sewer and storm water functioning, parks and playground site plans, neighbourhood theming, landscaping and building aesthetics and block and lot layouts.

Considerations specific to these lands needing to be addressed include:

- a) It is important to note that the infrastructure servicing strategy for the SEASP was based on Option A. In order for Option B or a hybrid between Option A and B to be the basis of an Outline Plan, additional engineering analysis is required (particularly in regards to storm water storage) during the Outline Plan process by the landowners and their consultants to determine what measures/solutions are needed to accommodate low density residential development.
- b) The lands north of 24 Avenue S are bounded by key transportation corridors both municipally and regionally. In order to maximize the benefit of these corridors to the City, businesses providing employment need to be located at key transportation system junctions and have access to transportation routes. The nature of these corridors can also have an isolating effect and therefore multi-modal transportation linkages such as cycle ways and pedestrian connections will be an integral component of an Outline Plan and will be designed to connect within and outside the larger community.

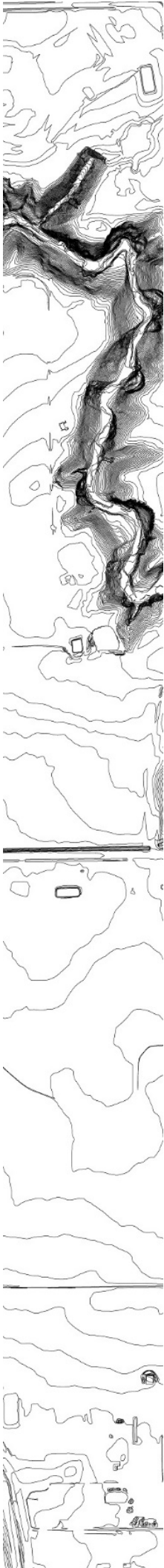
- c) Due to the existing physical constraints in the Special Planning Area, any concerns that the CP railway, irrigation district and/or natural gas pipeline companies may have concerning various types of development and their existing nearby infrastructure will be considered, addressed, and successfully mitigated.
- d) In terms of overall neighbourhood design and aesthetics, architectural controls will be developed to address the various building forms and give a sense of neighbourhood continuity and harmony between employment and residential uses. Any aesthetic, open space and recreation amenities that are incorporated into the development will be designed as inclusive and public in nature serving the needs of employees as well as potential residents. An overall landscaping master plan will be developed to provide screening and buffering between types of uses as well as to enhance the visual environment along the arterial roadways with the outcome of ensuring 24 Avenue S functions as a gateway to the City.
- e) Secondary or complimentary land uses must fulfill the needs of potential businesses, residents and employees in order for the area north of 24 Avenue S to feel like a complete neighbourhood.
- f) The outline plan must also include a section covering the overall community benefit of the proposed land use and how it will support the overall community vision adopted by City Council.

Preparation of an Outline Plan is at the landowner/developer's expense with the requirements of a governing Area Structure Plan, the ICSP/MDP and the SSRP needing to be met as well as City standards for infrastructure provision. The process for preparing an Outline Plan is indicated on the City's website and involves various stages of technical and neighbourhood design and review at each stage by City staff. If a disagreement on an engineering or technical aspect occurs the issue can be escalated for resolution. Final approval of the Outline Plan lies with the Municipal Planning Commission.

### Existing Recreation Facility

The existing recreation facility north of Highway 4 in the SEASP area, consisting of a 9-hole golf course, driving range, go-kart track and mini-golf, can operate indefinitely and has the opportunity to expand upon or upgrade their existing operations, but only by obtaining necessary approvals, such as zoning, development permits and building permits from the City of Lethbridge or any other required government agency. As the existing recreation facility does not require a significant amount of infrastructure, expansion of the facility may proceed prior to approval of an Outline Plan, provided this expansion does not require connection to City services, does not require improvements to the transportation system and does not vary from the existing recreation-type of uses. For example, the existing 9-hole golf course could likely be expanded to 18-holes prior to approval of an Outline Plan or the extension of City services to the property. However, the addition of a waterslide park would likely require full utility services and transportation upgrades to be in place first.





Over the long term, it is also possible that this recreation facility may be redeveloped for other purposes. In these circumstances, the site shall be developed in the same manner as the surrounding Special Planning Area. Redevelopment of this site also depends upon the provision of utility and transportation infrastructure.

#### 4.3.1 Objectives

- a) Provide for land uses that fulfill the vision of the ICSP/MDP.
- b) Provide for a unique land use mixture of services, businesses, public uses and residential forms of housing in the City.
- c) Allow the continued operation, possible expansion or transition of the existing recreation facility located north of Highway 4.
- d) When developing the land use composition, utilize access to national and international transportation routes.
- e) Ensure compatibility between all land uses within an Outline Plan area and adjoining or adjacent land uses.
- f) Ensure 24 Avenue S functions as a gateway to the City.

#### 4.3.2 Policies

- a) Land uses shown in Outline Plans will comply to the vision of the ICSP/MDP and the policies of this ASP.
- b) If preparing an Outline Plan for Option B, the developer/consultant shall adhere to the listed considerations of page 54.
- c) Measures to provide a buffer and transition between the Special Planning Area and residential land uses to the west and the south shall be determined at the Outline Plan stage for Outline Plans E1 and E2 (As identified on Map 15).
- d) Existing approved land uses in the Special Planning Area are permitted to continue to operate indefinitely in their existing capacities.
- e) The general categories of the land uses that were identified under Policies 4.3.2 a) and b) shall be further refined through the land use districts that are applied to the Special Planning Area. Suggested Land Use Bylaw districts for this area are identified in Appendix B.
- f) The existing recreation facility may be upgraded or expanded subject to receiving the necessary approvals from the City of Lethbridge and/or other required government agency.



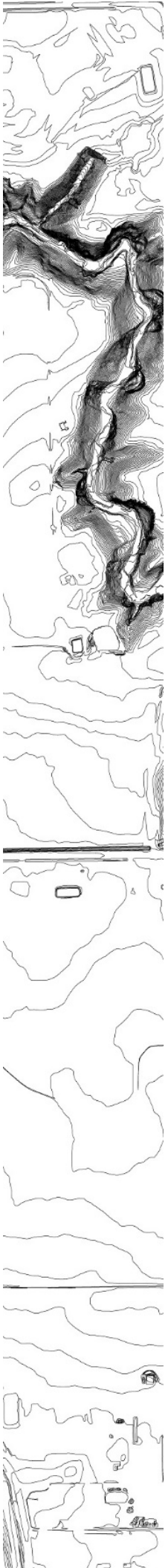
Redevelopment of this site, for uses other than those for recreation purposes, shall be developed in the same manner as the surrounding Special Planning Area and subject to the policies of Section 4.3 of this ASP.

- g) Activities that occur in the Special Planning Area shall not have an adverse impact on adjacent neighbourhoods due to noise, odours, outdoor lighting or emissions.
- h) Parking or storage areas shall be screened from public roadways and adjacent uses using a combination of landscaping or physical structures (if appropriate). Policies around screening in this area shall be further refined at the Outline Plan stage.
- i) If, due to the composition of land uses and the desired aesthetic result, a new land use district other than what is already found in the current Land use Bylaw is required, a draft will be included within the applicable Outline Plan.
- j) Access and facilities for disabled persons shall be provided in accordance with the City of Lethbridge Land Use Bylaw and the Alberta Building Code. The “Barrier-Free Design Guide” published by Government of Alberta Safety Code Council provides further explanation as to the intent of the Alberta Building Code in this regard.
- k) The use of innovative technologies or practices that are energy efficient, reduce waste or are environmentally responsible in the development of the area, and can be addressed through existing City of Lethbridge standards or the Alberta Building Code, shall be considered favourable. Elements can include, but are not limited to:
  - Green building practices (i.e. LEED)
  - Onsite energy generation
  - Eco-Industrial Parks
  - Industrial Symbiosis

#### 4.4 Community Nodes

Within the SEASP area, the Community Nodes are intended for higher intensity development that serve the surrounding neighborhood and, in some instances, the entire City. The Community Nodes will be public gathering places where residents go for recreation and amusement, shopping, health care and personal needs. Medium and high density residential development will also occur at the Neighbourhood Nodes to take advantage of proximity to this mix of amenities.





## “What attracts other people most, it would appear, is other people.”

*William H. Whyte*  
*Urban Journalist & Author*

Due to their importance to residents as “destinations”, the Community Nodes will also function as transportation hubs. Each Community Node will be positioned on a transit route and will have at least one transit stop integrated into the Node. Connecting the Community Nodes to active transportation modes are particularly important in that residents can frequent them by cycling or walking. Each Community Node will be connected to the pathway and sidewalk system. To provide efficient vehicular access, the Community Nodes will also be located adjacent to major roadways (collector or arterial roads). This will also help the marketability of the commercial uses contained in the Community Nodes and will ensure that commercial services are available to nearby residents in an expedient manner.

In the SEASP area, there are three Community Nodes which can contain the following uses:

- Large Format Retail Commercial
- Neighbourhood Commercial
- Medium Density Residential (37-75 dwellings per net hectare)
- High Density Residential (Over 75 dwellings per net hectare)
- Public Facilities
- Religious Assembly
- Child Care Facilities
- Mixed Use
- Open Space

The three Community Nodes, including what land uses will occur at each specific Community Node, are described in greater detail below and shown on Map 7.

**Community Node 1.** This Community Node is located at the junction of Highway 4 and the future north/south spine road that connects Highway 4 to the centre of the SEASP area. As such, this node enjoys high visibility and is located next to active and vehicular transportation routes that have been designed to accommodate high volumes of traffic. This node will primarily contain large format retail development that serves the broader overall City (potentially the region), comprise large structures/developments and will require ease of access

to major regional transportation routes. Examples of large format commercial development can include lifestyle centres, department stores and hotels.

This node can also contain major public facilities; potentially an ice rink, pool or cultural facility that will serve the population of the south sector of the City. Due to the nature of these uses, it is expected that public facilities will require a large area of approximately 4–6 hectares (10–15 acres) at this Community Node. Multi-dwelling residential, consisting of medium or high density development would be complimentary at this node. A park and linear open space along the canal and a stormwater pond are adjacent to Community Node 1. This node will relate to this nearby open space and the contained pathway by offering uninterrupted pedestrian connectivity between the two areas. The opportunity will also exist for land uses contained within this node to take advantage of the nearby open space and canal. For example, a restaurant or café found at this node could include outdoor dining that fronts onto the adjacent open space.

Religious Assembly and Child Care Facilities could also be located in Community Node 1.



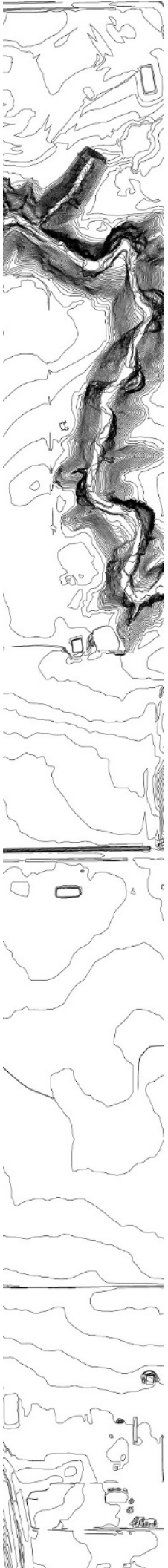
*Example of mixed use development*

**Community Node 2.** This Community Node is the focal point of the SEASP. It is intended to set a new standard for the City by creating a place where people can gather, build relationships and participate in civic life. This node is envisioned as a public open space or park containing a stormwater management facility surrounded by medium or high density residential, as well as commercial and mixed use development. Outside central areas of the City, there are no similar public spaces available in Lethbridge at this time. The intent is to recreate this same opportunity in the SEASP area.

To encourage higher density development at this Community Node, building forms will be a minimum of two storeys in height. Commercial development can consist of retail shops and offices. Public uses, serving the surrounding neighbourhood population, such as a community centre, child care facility, religious assembly or other complimentary uses will also be encouraged at this location.

This Community Node will be well connected to other development and open space through roadways, but also, through extensive pathways and linear open spaces that form the park and open space system in the plan area. The central public open space in this node is one of the key elements in the overall park and open space system. To truly set it apart, it will have





an aesthetic that is more urban in nature than other suburban parks, but will still contain a combined water feature/stormwater management facility. Further details on this public open space are described in Section 5 of this ASP.

**Community Node 3.** Community Node 3 will primarily serve the commercial needs of residents in the far south of the plan area. These residents will be separated by distance to the other commercial uses in Community Nodes 1 and 2 and the large format commercial area on Mayor Magrath Drive. This will consist of neighbourhood commercial development that will primarily serve the local neighbourhood population and comprise relatively small commercial structures that are found within neighbourhoods. Examples of neighbourhood commercial development can include gas stations, convenience stores, car washes, coffee shops and small offices. Multi-dwelling residential, consisting of medium or high density development, is appropriate at this node, as are religious assemblies and child care facilities. Public uses that serve the surrounding neighbourhood population, such as a community centre will also be encouraged at this location. Public open space will be found adjacent to this community node. Additional details in terms of land use and urban design for the Community Nodes will be determined at the Outline Plan stage.



*Example of neighbourhood commercial development in Sunridge, West Lethbridge*

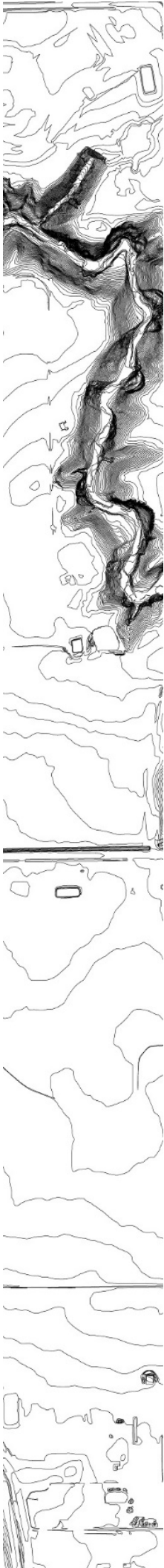
#### 4.4.1 Objectives

- a) Maintain Community Nodes in the SEASP area for higher intensity development and public gathering spaces that serve the surrounding neighborhood and broader City.
- b) Ensure that the Community Nodes are well connected to the City-wide transportation network.
- c) Establish a place for people to gather, build relationships and participate in civic life at Community Node 2.

#### 4.4.2 Policies

- a) The potential allowable land uses at each Community Node are specified as follows:
- Community Node 1 – Large Format Retail Commercial, Medium Density Residential, High Density Residential, Mixed Use, Cultural Facilities, Sports and Recreation Facilities, Religious Assembly, Child Care Facilities
  - Community Node 2 – Neighbourhood Commercial, Medium Density Residential, High Density Residential, Mixed Use, Open Space, Cultural Facilities, Sports and Recreation Facilities, Religious Assembly, Child Care Facilities
  - Community Node 3 – Neighbourhood Commercial, Medium Density Residential, High Density Residential, Mixed Use, Cultural Facilities, Sports and Recreation Facilities, Religious Assembly, Child Care Facilities
- b) These land uses shall be further defined at the Outline Plan stage and in conjunction with the appropriate land use districts in the City's Land Use Bylaw at the time of development.
- c) The Community Nodes shall incorporate a parking maximum of 25m<sup>2</sup> of gross floor area per stall for commercial buildings over 2,000m<sup>2</sup>.
- d) On-site bike parking shall be provided for all land uses contained in the Community Nodes. This specific requirement shall be determined prior to the Outline Plan stage. This requirement shall identify the minimum number of stalls that are required in addition to other requirements, such as the location of parking stalls on a site or the parking structure that should be used (bike rack etc.). The specific bicycle parking requirement shall be determined based upon an investigation of the bicycle parking requirements for similar land uses in similar cities.
- e) Transit stops shall be located at each Community Node.
- f) Each Community Node shall have strong pedestrian and cycling connections to other focal points within the community to encourage active transportation to and from the Community Nodes.
- g) Each Community Node shall include or be adjacent to public open space.
- h) Each Community Node shall interface with adjacent open space through uninterrupted pedestrian connectivity. Provisions will be made for land uses, found in each Community Node, to take advantage of their proximity to this open space. This will be further detailed at the Outline Plan stage.



- 
- A vertical topographic map on the left side of the page shows a river valley with contour lines and some buildings. The river flows from the top left towards the bottom right.
- i) Community Node 2 shall incorporate comprehensive design requirements that deal with the aesthetics and functionality in terms of the overall design. These requirements will be described further at the Outline Plan stage.

## 4.5 Gateway Features

As the eastern portion of the plan area, along Highway 4 (24 Ave S) serves as a major gateway function into the City, gateway features (a minimum of one) shall be located at the locations shown on Map 7. The extent of these features can range from a static feature that does not require any full time resources, such as a historical monument (see section 2.4.4 or 2.4.5), or a feature that includes different facilities and is staffed by permanent employees, such as a tourist information bureau. The type of gateway feature(s) to be developed at this location will be further determined through the future requests and needs of the City and partnering groups, such as the Chinook Country Tourist Association.

### 4.5.1 Objectives

- a) Provide a gateway feature(s) at the eastern boundary of City, adjacent to Highway 4.

### 4.5.2 Policies

- a) For the development of gateway features into the City, one to two sites shall be provided near the eastern boundary of the City, adjacent to Highway 4. The nature of these sites can be determined at the Outline Plan stage or sooner if necessary.
- b) Gateway features will be considered to range from static information displays, such as monuments to fully staffed facilities, such as a tourist information centre.

## 4.6 Schools

A minimum of two school sites will be provided in the SEASP area, with the potential of an additional third school site provided in the far south if warranted by future demographics. Connecting these schools to active transportation modes is important to encourage parents and students to commute to and from school by cycling or walking. Each school site has also been located on a collector roadway to facilitate school bus access and parent drop-off/pick-up.

The school site located in the western portion of the plan area is required in the near future and will be one of the first facilities developed in the SEASP area. This school was first planned to be located in the former Southgate ASP, however, the land was not available when needed by the Public School District. As indicated by the school district, the new school site provided in the SEASP is in a better location than the previously planned school in the Southgate ASP and will serve students who will live in the SEASP area, as well as those who live in south Lethbridge's existing neighbourhoods. In order to facilitate its expedient development, this school has been located near existing development in Fairmont and Southgate/Coulee Creek, where utility and transportation infrastructure are most readily available.

A second school site has been provided in the centre of the SEASP area. This site is equally accessible to all residential areas in the SEASP plan using transit, active transportation modes and vehicular transportation modes.

A third potential school site has been provided in the far south of the plan area for future residents in this area to utilize. It is unknown at this time whether this school will be required, as the surrounding area will be one of the final areas to be developed. Demographic projections conducted for this ASP indicated that student generation in the immediate area may be sufficient to support a small school that is below the standard school size typically used by the school districts. However, this depends on number of factors, including how fast the SEASP develops overall, the school standards that are used by the school districts at the time of development and what occurs in other schools elsewhere in Lethbridge. The Outline Plan drafted for the area containing this potential school site will further assess the need for this school site or any other additional schools sites at the time it is drafted.

The exact location of each school site will be determined at the Outline Plan stage. Current standards for school site sizes are located in the Technical Documents Appendix.

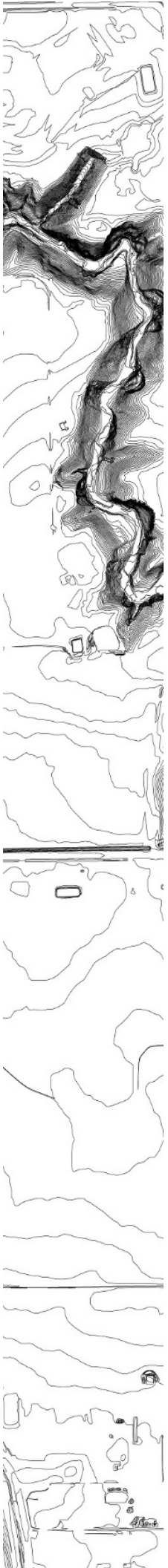
#### 4.6.1 Objectives

- a) Maintain a sufficient number of suitable school sites to provide educational services to residents in the overall community and the SEASP area.
- b) Encourage the utilization of a transportation network that encourages students and parents to be active.

#### 4.6.2 Policies

- a) A minimum of two school sites shall be provided in the SEASP area, with a third potential school site included in the southern portion of the plan area.
- b) The first school in the Area Structure Plan shall be considered in the area east of Fairmont and Southgate / Coulee Creek.
- c) The exact location, size and layout of the school sites will be determined in further detail at the Outline Plan stage in conjunction with the appropriate school district.
- d) The need for a third school site, or any additional school sites in the southern portion of the plan area beyond what has been considered in this ASP will be further assessed at the Outline Plan stage.
- e) Each school site shall have strong pedestrian and cycling connections to other portions of the SEASP area and the overall community to encourage commuting to and from school using active transportation.





## 4.7 Semi-rural

The Semi-Rural area is the portion of the SEASP area that is isolated from the rest of the City due to its location north of the railway, and the fact that it is surrounded by Lethbridge County on three sides. Future urban development, consisting of full services and infrastructure, is severely hindered by the following issues:

**Access.** Access to this area would require crossing the railway to the south. There is an existing at-grade crossing to the south, however, for any significant development north of the railway, at least one, possibly two, above-grade crossings would be required to be constructed. A 2015 order of magnitude estimate calculated the capital cost of constructing above grade crossings at approximately \$15 million each. It is not cost-effective to construct an above-grade crossing to access an area of approximately 75 hectares.

**Drainage.** The Semi-Rural area contains the lowest part of the SEASP area and is the natural direction for drainage in this part of the City. Storm water runoff becomes trapped in this area due to the low elevation and the location of the railway. The area east of Lethbridge, towards Coaldale, known as the Malloy Drainage basin, experiences significant drainage issues. Increasing the amount of developed hard or non-permeable surfaces in the area would increase stormwater runoff, further contributing to the drainage problems that this area and the adjacent County already experience.

**Utility Servicing.** Servicing to this area to allow for any significant development would require major utility pipes to cross under the railway. In addition to crossing the railway, any storm and sanitary sewer services would have gravity to overcome, as this area is lower than the areas to the west and south. These services would need to be pumped, against gravity, to an area west of the railway to where they could enter the larger overall system. A 2015 Order of Magnitude estimate calculated the capital cost of constructing one sanitary sewer lift station and one storm sewer lift station at a total of \$9 million. This does not include the cost to extend water, sanitary sewer and storm sewer trunk lines to this area, which was estimated to be an additional \$3 million cost.

The capital costs to construct the necessary systems required to adequately service this area to facilitate full development would be the responsibility of the City of Lethbridge through the Offsite Levy. These costs are not practical or financially sustainable, nor are the ongoing maintenance, operational and eventual replacement costs of such systems for an area of this size.

Despite these limitations, there is the possibility for the Semi-Rural Area to contain some form of low-intensity development that is relatively rural in nature. Such development must maintain the majority of the area for open space, not require additional infrastructure (i.e. sanitary sewer, storm sewer, water and transportation), restrict additional stormwater runoff and limit future subdivision. Examples of potential uses that may be acceptable include: accessory buildings, agricultural services, agricultural research facilities, auction sales, garden centres /greenhouses, golf courses / driving ranges, landscaping services, outdoor motorsports



and recreation, outdoor and recreational vehicle storage, riding academies and arenas, shipping container storage, seasonal campgrounds and veterinary clinics.

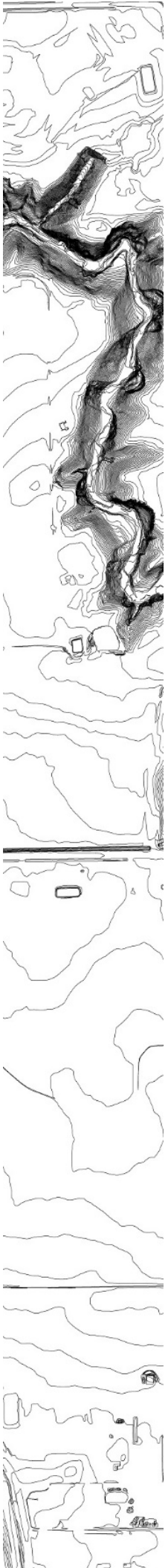
#### 4.7.1 Objectives

- a) Maintain the rural nature of the area north of the railway in the SEASP area.
- b) Limit future development, north of the railway, to that which requires no additional infrastructure servicing.

#### 4.7.2 Policies

- a) Future development that occurs in the Semi-Rural area, north of the railway, must be considered low intensity or relatively rural in nature by the development approval authority for development to be approved.
- b) Development that occurs in the Semi-Rural area shall not require additional utility services, such as water, sanitary sewer and storm sewer.
- c) Development that occurs in the Semi-Rural area shall not require upgrades or expansion to the existing road network and shall not require improvements to the existing 58 Street S railway crossing.
- d) Future development that occurs in the Semi-Rural area shall not add to the storm water run-off that already occurs. Additional stormwater from any future development in this area shall be captured and retained on the same parcel.
- e) If a proposed development is neither a permitted nor a discretionary use under the existing land use district for a given parcel, an application to amend the Land Use Bylaw shall be required. Such an application must be approved by Lethbridge City Council prior to the development of the intended use.
- f) Any Land Use Bylaw amendment proposed to facilitate development in the Semi-Rural area must redistrict the land to a Direct Control (DC) district. This district will place specific limitations on the proposed development to ensure that this development maintains its rural nature. Conventional zoning districts do not allow this type of control.
- g) Standard procedures for Land Use Bylaw amendments or development permit applications and any subsequent public notification or public hearings shall still apply for any development proposals in the Semi-Rural area. The approving authority shall retain the ability to approve, amend or refuse any such application.
- h) A stormwater management plan will be required for any new developments in the Semi-Rural area showing the amount of non-permeable surfaces that will be utilized and how the proposed development will mitigate or reduce downstream impacts. The plan will be subject to comment/review by the County of Lethbridge and the City of Lethbridge shall





retain the ability to approve, amend or refuse an application based upon the stormwater impacts of the proposed development.

- i) The maximum amount of land that can be developed in the Semi-Rural area is at the discretion of the development authority. It shall be based upon such factors as existing roadway capacity, existing servicing capacity or emergency services response and the cumulative effect that prior development has had. This maximum has been applied to ensure that any existing limited services are not used at a greater level than what they are designed for.
- j) The City shall maintain the ability to impose any other restrictions through the Direct Control land use district or through any development permits that are issued to facilitate additional development in the Semi-Rural area. As they are general in scope, the policies regarding interim land uses that have been laid out in this ASP cannot effectively regulate all aspects of a development. Restrictions that are imposed through the Direct Control land use district or development permit are site-specific to a particular proposed development.
- k) Additional subdivision of existing parcels shall be restricted, unless it is proven by the applicant that the proposed subdivision will not be a detriment to maintaining the rural nature of the area north of the railway, and is allowed in the Direct Control district that is specific to the site.
- l) Existing land uses and structures that have previously been established in the Semi-Rural area shall be permitted to continue to operate as they have in the past. Expansion of existing operations are as governed under the existing land use district for a given property.
- m) Any development that occurs in the Semi-Rural area shall be subject to an Outline Plan. As this area will receive no additional utility services, this Outline Plan may be drafted and approved at any time, independent of development occurring elsewhere in the SEASP area.

#### 4.8 Undevelopable Land

An area of undevelopable land exists in the far southeast corner of the SEASP area. This area is considered undevelopable, as it is located in a low spot near the junction of the existing area at 58 Street S and 60 Avenue S. This area captures storm-water runoff from the north and through a culvert from Lethbridge County lands south of 60 Avenue S. The size of this particular area and its location to



*Undevelopable land at the junction of 58 Street S and 60 Avenue S after a heavy rainfall*

make it uneconomical to develop. The future curve in the adjacent arterial roadway also makes access to this area undesirable. This area is to remain in its natural state, however, the potential exists for it to become part of an engineered wetland. This could be a site where additional wetland is provided to replace wetland that is lost to development, as specified in the Province's *Water Act*.

#### 4.8.1 Objectives

- a) Prohibit the development of the area shown in this ASP as Undevelopable Land.
- b) Consider utilizing this area as an engineered wetland.

#### 4.8.2 Policies

- a) Development shall be prohibited in the area shown in this ASP as Undevelopable Land.
- b) Utilizing the area considered as undevelopable for the purpose of providing replacement wetland for wetlands that have been developed shall be further investigated at the Outline Plan stage and in conjunction with the Province's *Water Act*.



## 5.0 Park & Open Space System

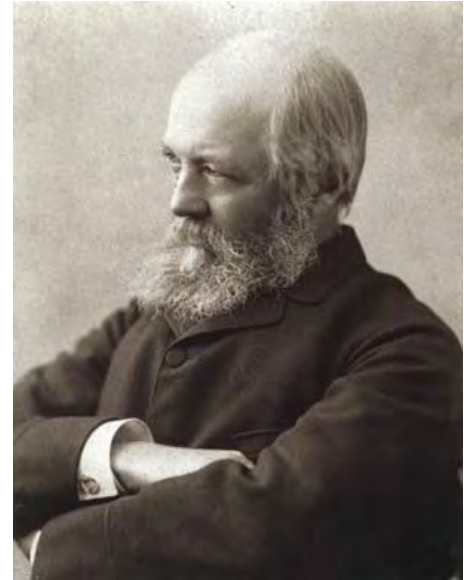
The Park and Open Space System is the quintessential feature of the SEASP area. In the spirit of the ideals of the preeminent landscape architect Frederick Law Olmsted, the varied types of open spaces contained in the SEASP area shall be considered together as a **functional system**, as opposed to individual entities. All of these public spaces must coherently function together in a network connected to one another by pathways and cycleways. The SEASP area is envisioned as an area of enhanced multi-modal transportation functionality, with a variety of open and park spaces that foster vibrancy and connection. Like other recently adopted ASPs in the City, the SEASP is also seen as an opportunity to promote greater attention to public realm design.

It is the overall goal of the Park and Open Space system to allow patrons to have a range of experiences and connect the organic, natural environment of Six-Mile Coulee with the purposefully designed and landscaped environment of Henderson Lake Park. Through this paradigm, there will be opportunities to play, learn, embrace natural landscapes, exercise, connect with others and participate.

In the design concept for the open space and park system, a number of themes were considered:

**Functionality.** Park and open spaces must be functional. This can be achieved by anticipating and connecting high traffic spaces and uses. Open Spaces and Parks should serve as destinations in their own right, but should also be tools for connectivity.

Functionality also describes the types of activities that can occur in public open spaces. These can include planned activities, such as organized soccer leagues or spontaneous, informal activities, such as pick-up basketball games or tennis. Activities can also include those that are active in character, such as skateboarding or those that are passive, such as walking or picnicking. Through the planning process it has been identified that Lethbridge has sufficient facilities for organized outdoor recreational events, but is lacking places that are open to the general public for informal activities to occur. It was also identified that Lethbridge also has an abundance of playgrounds. The SEASP area will include open space for planned, organized activities and playgrounds but will focus on areas where spontaneous play and recreation can occur. Potential facilities can include, outdoor basketball courts, ball hockey courts, skateboard parks, and small, drop-in sports fields, among others.



*Frederick Law Olmsted is credited as the founder of American Landscape Architecture. Olmsted designed New York's Central Park, the Grounds of the U.S. Capitol in Washington, and the Buffalo Parks System in Buffalo, NY.*  
Source: National Association of Olmsted Parks



*Inspired by the squares and boulevards of Paris, the Buffalo, NY, park system is the oldest and best example of a coordinated park system in the U.S., and is on the National Register of Historic Places.*  
 Source: The School of Architecture and Planning, University of Buffalo SUNY

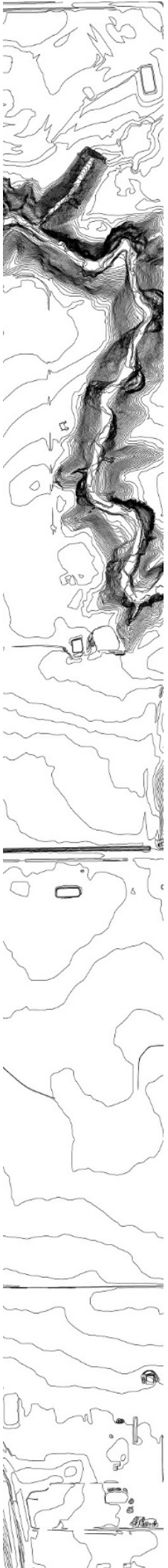
**Safety & Accessibility.** Park and open spaces should be designed to serve all potential users, not just the majority. Appropriate design of spaces includes considerations for promoting use by all age groups and mobility levels. Design should also incorporate best CPTED practices.

**Place-making.** Park and open spaces serve an important function within neighbourhoods as tools for place-making and community-building. The design of open spaces and parks can foster these outcomes through high quality public realm design and by encouraging a variety of activities. Creating meaningful places also requires that places be distinct from one another, while still reflecting the nature of nearby land uses and context. Examples of how to create diversity include: using different sizes of parks and mixing manicured and natural landscaping. The element of wonder should not be overlooked in park design as a way to create a special place.

**Sustainability.** Park and open spaces can be used strategically to protect and enhance sensitive environmental and historic landscapes. They can also be leveraged to support educational outcomes by demonstrating the importance of ecosystem services. Sustainability is also enhanced through good open space and park design and maintenance.

**Community Benefit.** Parks can vary greatly in terms of maintenance costs depending upon what facilities they contain. It is intended to maximize the functional value of parks and open spaces where possible in the SEASP. For example the development of a park containing a number of facilities, such as playgrounds, pathways and sports fields is preferred over a park that has similar maintenance costs, but contains few recreational opportunities that benefit the overall community, other than walking or running.





Each of these themes is important in its own right, however each requires that the others be present in order for all themes to truly materialize—for example, place-making is only possible if the places are safe; positive health outcomes are much more probable if a pathways lead somewhere interesting. It is also important to realize that these themes are aspirational, and that not every open space and park is going to achieve all of these goals perfectly, all at once, or even in the short-term. Themes like place-making happen over a number of years. But this is an outcome worth striving for. Figure 10 represents this complex interplay of inputs, users and outcomes.

## SEASP open spaces shall be considered as a *functional system*

Within the SEASP area, there is expected to be approximately 53 ha of land available for the purpose of parks, open space and

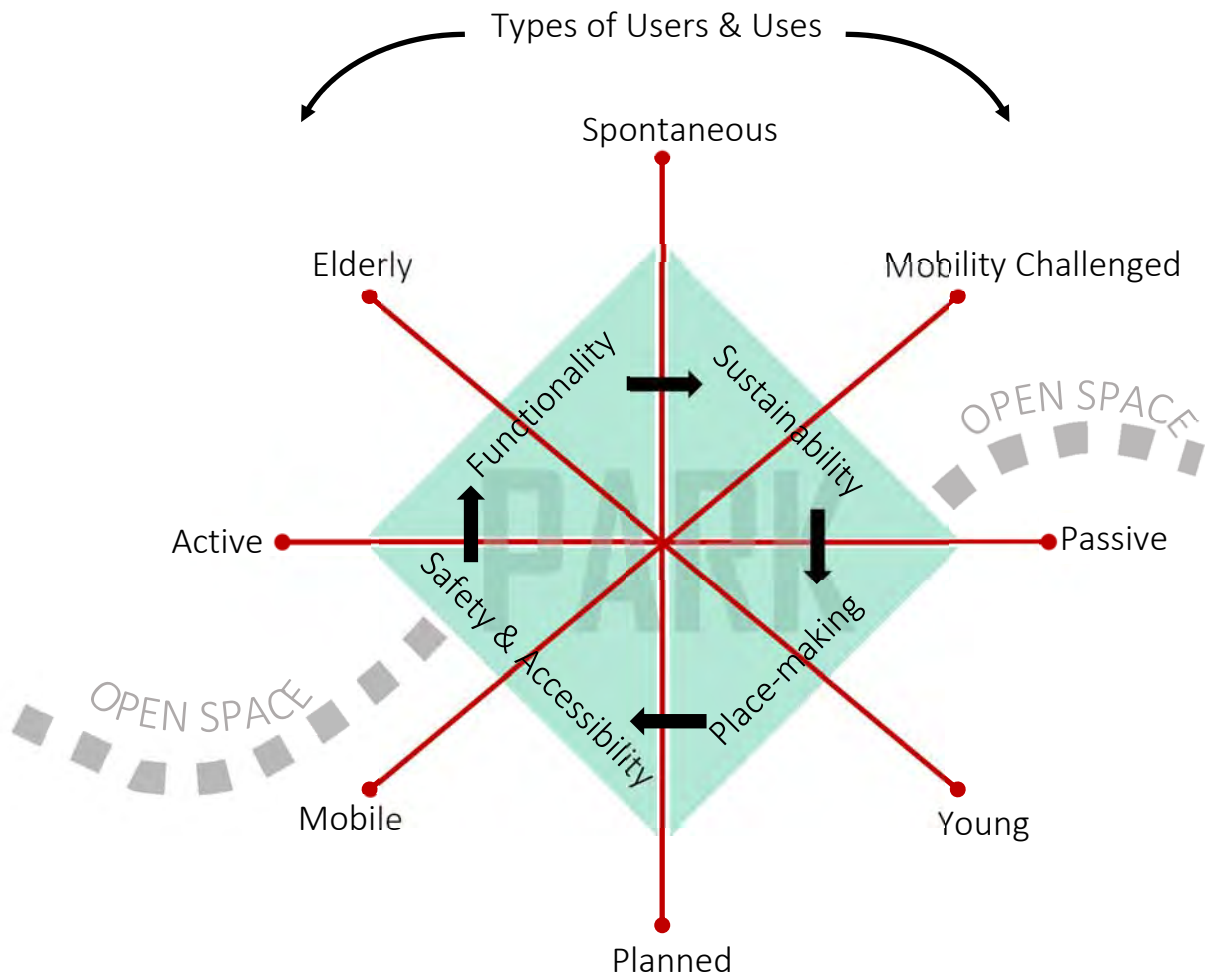


Figure 10 Park & Open Space System Considerations

schools (not including Environmental Reserve). This averages approximately 3 ha / per thousand residents when the plan area achieves its maximum build-out.

## 5.1 Park and Open Space Concept

It should be noted that the SEASP does not show all park and open spaces. Rather, it focuses on major open spaces and parks. Smaller neighbourhood scale parks and open spaces will be defined at the Outline Plan stage when more details with regard to block pattern and other land uses are known.

The open space system within the SEASP includes a combination of parks, school sites, stormwater management facilities and linear open spaces. Open spaces will compliment and integrate with the mix of residential, commercial and business industrial land uses, creating an network of play, live and work spaces (see Map 8).

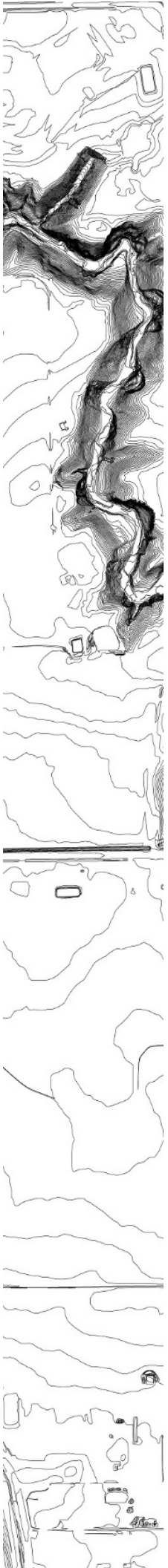
The key open space linkages will preserve public access, buffer non-compatible land uses, enhance ecosystem services and water management and support active multi-modal transportation. Echoing the key themes discussed above, the open space concept promotes functionality, safety and accessibility, place-making and sustainability.

There will be numerous open and park spaces of all sizes throughout the plan area. This section describes key open and park space features and nodes as a way of guiding subsequent Outline Plans. While not all open and park spaces are specifically identified in this ASP, the Objectives and Policies contained in Sections 5.1.1 and 5.1.2 shall be adhered to for all open and park spaces.

**Municipal Reserve.** Much of the open space within the SEASP area will be provided by Municipal Reserve as specified in the Municipal Government Act. Within subsequent Outline Plans, the specific location and area to be dedicated as Municipal Reserve will be identified. Outline Plans will be required to create functional networks of open space that are linked to existing and proposed neighbourhoods in southeast Lethbridge, which may be included as part of Municipal Reserve. Municipal Reserve may also be a strategic tool used by future developers to balance land uses which are less compatible, e.g., residential and commercial uses and multi-family residential and business industrial uses. Within the plan area, Municipal Reserve will also be allocated for future school sites.

**Stormwater Management Facilities.** When development takes place in areas that have previously been undeveloped, there is always the likelihood of disturbing natural wetlands. The Biophysical Impact Assessment identified numerous wetland locations within the plan area that must be carefully considered within the context of development. These areas will require additional study and classification at the Outline Plan stage (see Section 2). Current practice in the City of Lethbridge is to mitigate to the greatest extent possible the removal or degradation of existing wetlands, and where feasible, integrate these areas into the open space system or as stormwater management facilities. Wetlands that are designated as such may be eligible as Environmental Reserve.





It is important to keep in mind however that when a wetland is converted into a stormwater management facility it will have reduced functionality within the larger ecosystem, in terms of water absorption, flood attenuation, biofiltration and habitat. Constructed wetlands or wetlands that become stormwater management facilities primarily serve a utility function, despite the aesthetic qualities they have as open space elements and the important ecological role they may still play.

Where these facilities are integrated into open spaces, they will be accompanied by a sufficient level of landscaping that is compatible with development in their immediate vicinity but which do not compromise their utility function. Landscaping will follow Parks Department best practices and standards.

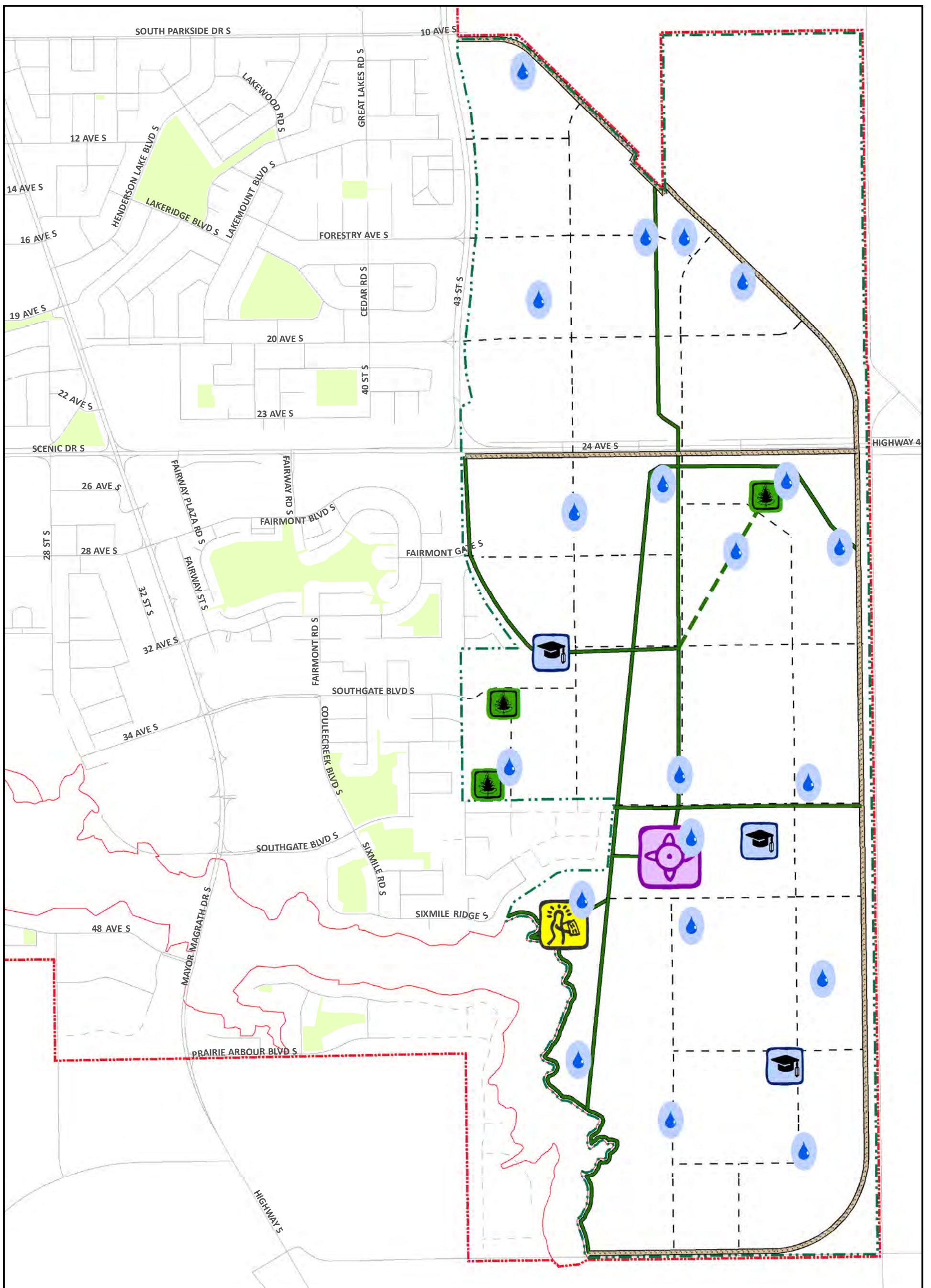
**Environmental Reserve.** Section 664(1) of the MGA establishes subdivision authority's ability to require Environmental Reserve dedication at the time of subdivision in a number of situations. Of specific relevance to the SEASP, the MGA allows Environmental Reserve to be taken if land consists of a coulee or body of water. The MGA also allows for land to be dedicated as Environmental Reserve to protect an area from pollution and to ensure public access to Reserve sites.

Given the proximity of the plan area to Six-Mile Coulee, and the presence of a number of wetlands, Environmental Reserve dedication will be required prior to development. Subsequent Outline Plans shall indicate specific Environmental Reserve dedications in line with the MGA and the River Valley Area Redevelopment Plan's (or successor document) specification for development setbacks along Six-Mile Coulee. Areas below this development setback will be dedicated to the City of Lethbridge as Environmental Reserve. In accordance with Alberta's *Water Act*, existing wetlands will also be dedicated as Environmental Reserve. This dedication shall be reviewed and approved by the subdivision authority at the time of subdivision.

**Six-Mile Coulee Trail Linear Open Space.** Given its environmental and cultural importance, a band of linear open space is incorporated into the open space concept as a method of protecting Six-Mile Coulee from neighbouring residential development. The band of linear open space will follow the safe development setback line along the top of bank. Buffering the top of bank area will:

- Mitigate the risk of erosion and sloughing on residential properties;
- Provide physical public access through park space and a pathway system to Six-Mile Coulee;
- Create a sense of public ownership over the land;
- Protect sensitive native plant and animal species; and
- Protect an important Blackfoot traditional use site.





**Legend**

City Boundary	Arterial Pathway	Interpretive Park
Southeast Area Structure Plan	Park Pathway	Open Space
Existing Parks	Potential Connection	Public Square
Setback Line	Stormwater Facility	School
Existing Roads		
Planned External Roads		
Proposed Road Network		

## MAP 8 - Open Space And Pathways

The proposed open space and pathways design for the Southeast area focuses on connectivity, large amounts of open space, and creating natural, safe spaces for social gathering and interaction. The markers in this map indicate the proposed locations of parks, open space, stormwater facilities, and pathways, however this list may not be comprehensive nor indicative of actual size. Subsequent outline plans will determine exact locations, sizes, and designs.





The linear open space will be constructed as specified by the Parks Design Standard for linear parks at the time it is created and will extend along the top of the entire east edge of Six-Mile Coulee within the plan area, parallel to the safe development setback line.

The Six-Mile Coulee Trail Linear Open Space will feature landscaping (including the restoration of) utilizing native plant species and park furniture such as benches that can be used by residents and visitors. Lookouts will be strategically located within the linear open space offering views of Six-Mile coulee and beyond. Finally, the linear open space will integrate with the interpretive trail system within the Blackfoot Interpretive Park, discussed in section 5.2.

The Outline Plans containing this area (OP areas R1b and R3 - see map 15) will demonstrate public access to Six-Mile Coulee in the design of roadways, block layout and lot layout.

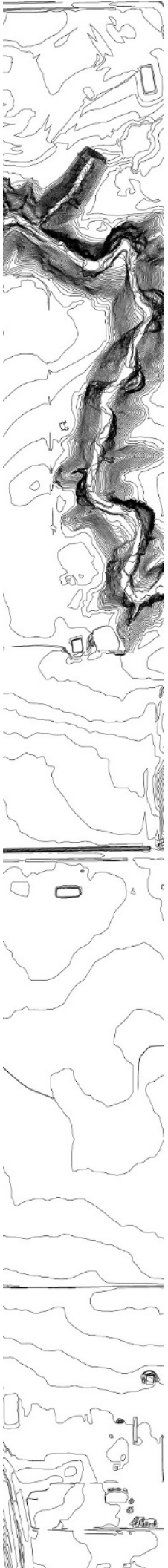
In the event of a wildfire, this band of public open space on the ridge of the coulee also has the advantage of providing a break between the grass and brush in Six-Mile Coulee and private dwellings.

**SMRID Canal Linear Open Space.** Irrigation has played a central role in the narrative of Lethbridge's growth as a regional hub for agriculture and agri-business through much of the twentieth century. Without irrigation, much of the cropland surrounding the City would not have the same potential, nor is it likely that the population of the City would have maintained such steady growth over the past century.

Lethbridge is surrounded on all sides by Irrigation Districts (IDs), including Lethbridge Northern ID (LNID), the St. Mary's River ID (SMRID), Taber ID and the Raymond ID. These ID's reflect the geographical extent and significance of irrigation in our region. Beyond agriculture, irrigation provides water for ponds located in the neighbourhoods of Fairmont and Uplands, as well as recreational areas such as Henderson Lake Park and Evergreen Golf Course.

SMRID has been in operation since irrigation first arrived to the area in the early 1900's. Today it is the largest ID in Canada operating over 2000km of canals and pipelines. SMRID canals continue to traverse parts of the City, including the SEASP area. While many of the





canals in the plan area are no longer in operation, one lateral canal that serves farmland to the north and east of the City crosses the northeast portion of the plan area. As discussed in Section 2.3, it is not possible to alter the current alignment of this canal.

The vision of the SEASP is to protect the integrity of the operating canal by incorporating it into the open space concept for the plan area. Doing so will mitigate downstream impacts on irrigators, while allowing for the creation of a linear park that can be used to showcase the history and continued importance of irrigation in our City and region.

The SMRID Canal Linear Open Space will be nearly two kilometers in length, and will parallel a large extent of the canal as it crosses through the plan area. The Linear Open space will offer sites for active (pathway) and passive (pedestrian furniture) recreation. The Linear Open Space will serve an important functional role connecting higher density residential and commercial areas south of Highway 4 (24 Avenue S) with employment areas to the north.

The design of the SMRID Canal Linear Open Space must also consider public safety. As this area is formalized as a recreational space, public safety concerns, including swimming and jumping into the canal, must be considered and appropriate measures taken to educate the public about safe use of the area. Where necessary, signage and physical barriers may be used to discourage unsafe use in specific areas. Subsequent Outline Plans will address safety concerns with respect to the Canal to the satisfaction of the SMRID.

The presence of a linear open space along the irrigation canal also prevents encroachment by private property, thereby ensuring SMRID can access the canal for maintenance purposes.

**High-pressure Gas Line Linear Open Space.** Due to significant relocation costs, it is expected that the existing right of way for the high pressure gas pipeline that runs north to south in the plan area will remain in its current alignment. In addition, other major utility service trunks will run parallel to this pipeline. A landscaped linear open space that runs above these utilities, and parallel to them, will be provided to provide open space for pathways and other recreation opportunities in the middle of the plan area. This open space will also provide an aesthetic feature to conceal these utilities and make space available for maintenance access. A similar right of way for major utility trunks runs from Henderson Park to the river valley. This existing right of way is extensively landscaped and contains pathways, playgrounds, sports fields and a toboggan hill.

**“There was  
rejoicing  
among the  
citizens and the  
small  
[children] of  
Lethbridge  
found the  
flowing water a  
source of much  
delight...”**

*Excerpt from the Lethbridge Herald  
following the arrival of irrigation water  
to Lethbridge, September 1900*

It must be noted that future landscaping within the high pressure gas pipeline right-of-way will be in accordance with the requirements of the owner of the high-pressure pipeline (see Technical Documents Appendix) and will require the approval of the pipeline owner. Landscaping in this right-of-way may consist of small shrubs, but not trees or large shrubs, so the operator can access the pipeline for maintenance purposes. The remainder of this green strip, which is not part of the gas pipeline right-of-way, does not require these restrictions, but will still need to be landscaped in a way that allows access for repair and maintenance of the other utilities in the corridor.

**Former 43 Street S Linear Open Space.** A full 75 metre wide right of way for a portion of 43 Street S east of Fairmont was previously established. However, as this right of way is no longer needed for an arterial roadway, a portion of this right of way will become a linear open space that is used for active transportation. This open space will contain a pathway that connects to the major intersection of 43 Street S and Highway 4 (24 Avenue S) and will also provide a direct connection to the future northwest school site.

**Central Parkway.** The Central Parkway is a major north-south collector roadway running through the heart of the plan area. It is a gateway that will serve a critical role as a multi-modal link channeling connector and local roads within the plan area to and from Highway 4. It will also connect residential areas in the south with public, recreational, commercial and business industrial land uses in the center and north of the plan area. The Central Parkway is an important place-making feature that will define resident, worker and visitor experience within the plan area. Leveraging that role to create a positive, meaningful experience among residents, workers and visitors will be an important aspect of streetscape and public realm design.

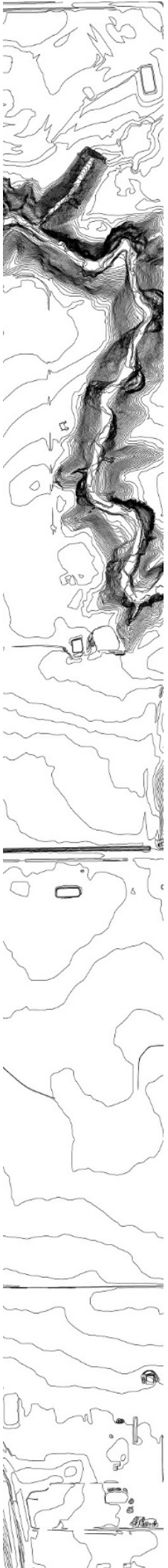


*Parkside Drive S in Lethbridge has many similar features that are also intended for the future Central Parkway*

Source: Google Earth Pro

The Central Parkway will be more than just a transportation spine for automobiles. It will serve as a multi-modal corridor for a variety of transportation options, including public transit, bicycles and pedestrians. The design of this corridor will endeavor to prioritize all of these uses equally. Encouraging multi-modal transportation in Lethbridge communities, as required through the ICSP/MDP, requires thinking beyond facilitating the movement of automobiles at the expense of other forms of transportation.





To create a positive, meaningful experience amongst users of the Central Parkway, this collector roadway will combine the public realm elements of a pathway system with the functionality of a major transportation route. This will require investments such as streetscape beautification, hard landscaping (i.e. decorative concrete), soft landscaping (i.e. plants), public art and the use of community entrance features. The design of the Central Parkway should endeavor to maintain a pedestrian-scale and pedestrian-friendly built-environment through the use of landscaping, small building setbacks, narrower road widths and intelligent crosswalk and intersection design that encourages foot travel over automobile traffic.



*Conceptual design of the future Central Parkway*

The Central Parkway shall also provide methods for cyclists to travel safely and swiftly through the plan area and should consider separating cyclists from vehicular traffic and pedestrians where possible. This could be accomplished by cyclist and pedestrians utilizing a multi-use pathway where cyclists and pedestrians are separated, or the introduction of cycleways (also known as cycle tracks), that physically separate cyclists from both vehicles and pedestrians and onto their own passageway. The further provision of cycling infrastructure, such as this, will be considered at the Outline Plan stage.

**Central Square.** A major community node shall be incorporated along the Central Parkway, approximately midway between Highway 4 and 60 Avenue N. As described in section 4.4, the node will be a core destination featuring a mix of commercial, public uses, medium and high-density residential and a central park. The node will showcase high quality public realm design, focused around a public square and stormwater management feature.



*Conceptual design of the future Central Square*

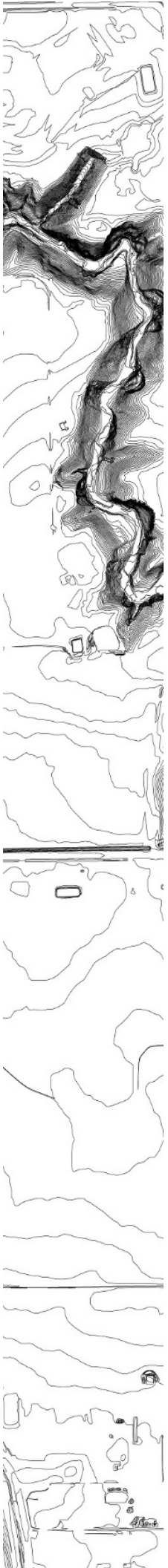
The Central Square shall facilitate place-making and community building in this area of Lethbridge by acting as an important gathering place for residents, workers and visitors of all age groups and mobility levels, as well as a hub for various forms of active and passive recreation. As a result, the use of hard and soft-landscaping, public art, pedestrian furniture and other design elements will be incorporated (such as a picnic shelter or gazebo). Examples of public realm elements that should be considered at the Outline Plan stage include: public monuments, murals, street-vendor and food truck infrastructure, bicycle parking, skateboard facilities, public art and water fountains.

**Parry Gardens.** A public park shall be established on a portion of the Parry Historical Site that contains the gardens. This site is included in Lethbridge's Heritage Inventory because of the existing house's unique construction style, the legacy of the original owner (Mr. Charles Parry) and the extensive landscaping and gardens on the site.

To try and maintain the original landscaping, as best as possible, any existing vegetation that is part of the gardens shall be utilized into the new park where appropriate. As well, any new vegetation that is planted as part of the park's development shall include a number of trees similar to those that the gardens were known for. To maintain the original application, as best as possible, and facilitate the transition of the gardens from private to public use. The establishment of a community garden will be encouraged in this location if one is to be developed in the SEASP area. Interpretative signage explaining the historical significance and function of the site will be included in this new park.

The existing house on the site will not be included in the park and will be subdivided onto a separate parcel.





### 5.1.1 Objectives

- a) Provide functional, safe and accessible open and park space within the plan area to foster a sense of place for residents, employees and visitors.
- b) Protect and mitigate impacts on significant environmental sites.
- c) Protect important Blackfoot traditional use sites.
- d) Conserve, to the greatest extent possible, natural wetlands within the plan area.
- e) Provide aesthetically pleasing stormwater management facilities that serve important utility functions and which may continue to support water management and wildlife refuge.
- f) Utilize the Central Parkway as north-south parkway through the plan area that serves the needs of pedestrians, cyclists, public transit and private vehicles.
- g) Incorporate a key community gathering place in the Central Square that contains high-quality public realm design features.
- h) Honour and communicate the original character of the gardens on the Parry Historical Site.



*Parry House and Yard*  
Source: Leo and Agnes Davidson

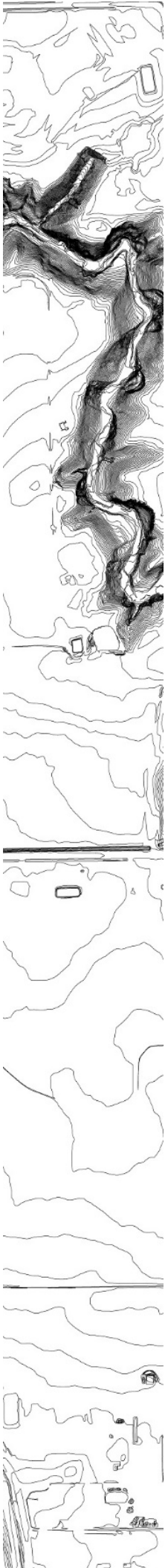
### 5.1.2 Policies

- a) Open and park spaces shall be located throughout the plan area and shall promote the principles of functionality, safety and accessibility, place-making, sustainability and community benefit.
- b) Parks and open space in the SEASP shall focus on providing opportunities for spontaneous, informal play and recreation to occur.
- c) The specific location of neighborhood scale parks and open space is not shown in the SEASP, but shall be determined at the Outline Plan stage.
- d) The amount of Municipal Reserve to be taken (including that for parks and schools shown in this ASP, and that for neighbourhood scale parks to be shown at the Outline Plan stage), shall be determined in accordance with the Municipal Government Act.
- e) The amount of Environmental Reserve to be taken shall be determined at the Outline Plan stage and shall follow the policies established through the River Valley Area Redevelopment Plan for lands along Six-Mile Coulee.



- f) Adequate land to accommodate stormwater infrastructure shall be provided in the plan area.
- g) In accordance with Alberta's Water Act, where possible, existing wetlands shall be avoided and dedicated as Environmental Reserve. Avoided wetlands shall be integrated into the Open Space and Parks System to ensure they are adequately recognized and protected.
- h) Where wetlands cannot be avoided, the developer shall demonstrate why avoidance is not feasible and appropriate compensation must be paid, as specified under the Provincial Water Act.
- i) Stormwater management facilities and wetlands shall be landscaped appropriately given their respective function and in consideration of adjoining land uses.
- j) Future Outline Plans will further determine where recreational uses can be safely integrated into stormwater management facilities.
- k) A buffer strip shall be provided along the top of Six-Mile Coulee for use as a linear open space. Further details of this buffer strip shall be provided at the Outline Plan and subdivision stages.
- l) The Six-Mile Coulee linear open space shall feature a pathway system along the east side of the Coulee that connects to other local open spaces, as well as parks to the east and north and to the Central Parkway.
- m) The Six-Mile Coulee linear open space will be integrated into the interpretive trail at the Blackfoot Interpretive Park (see Section 5.2).
- n) Public access to Six-Mile Coulee shall be demonstrated at the Outline Plan in terms of the design of the roadway, block layout and lot layout.
- o) The SMRID Canal Linear Open Space will parallel, to the greatest extent possible, the operational portion of the SMRID Canal within the plan area. Signage and interpretive way-finding shall be incorporated to reflect the history of irrigation in Lethbridge and region.
- p) Design of the SMRID Canal Linear Open Space shall minimize impacts on downstream irrigators and promote safe public use to the satisfaction of the SMRID.
- q) A Linear open space will run above and parallel to the existing High Pressure Gas Line right of way and the future proposed utility trunks in this area. This linear open space will provide different types of activities and the facilities to support them - such as pathways, sports fields and playgrounds.





- r) Any linear open space shall be a minimum of 15 metres in width and/or in conformance with the Parks Design Standard for linear parks that exists at the time it is constructed.
- s) Landscaping above the existing High Pressure Gas Line right of way shall require the approval of the owner of the pipeline and shall follow their requirements for development (see Technical Documents Appendix). Landscape requirements will prohibit large trees from being used and will necessitate the use of small plants/shrubs that are easy to remove.
- t) A Linear Open Space will exist in a portion of the 43 Street S right of way that has been established to the east of Fairmont. This linear open space shall be landscaped and contain a pathway for pedestrians, cyclists and other forms of non-vehicular transportation.
- u) The Central Parkway shall provide efficient and safe transportation options for motorists, users of public transit, cyclists and pedestrians. At the Outline Plan stage, design of the parkway shall consider dedicated cycleways, multi-use pathways and traffic-calming features such as raised crosswalks.
- v) The Central Parkway shall promote high-quality public realm design into the street-scape at a pedestrian-scale.
- w) The public open space located in the Central Square shall include hard and soft-landscaping, street furniture and active and passive recreation elements to encourage year-round gathering outside of local residents and workers. Examples include: public monuments, murals, street-vendor and food truck infrastructure, bicycle parking, public art, band shelters and water fountains.
- x) A portion of the gardens located on the Parry Historical Site shall be redeveloped as a public park that honours the original gardens by utilizing the existing vegetation and planting new trees similar to those that were originally on the site where feasible. This site shall utilize interpretive signage to describe its historical context.
- y) The development of a community garden, in the future park to be located on the Parry Historical Site, shall be encouraged.



Example of a public monument



Example of historical signage

## 5.2 Blackfoot Interpretive Park

Prior to the arrival of Europeans, the area in and around Lethbridge was, for hundreds generations, used and occupied by the Blackfoot peoples. The oral tradition of the Blackfoot has rendered the landscape and topography of the region into a rich tapestry of storytelling indicative of traditional land use. In particular, the area at the confluence of the Oldman and St. Mary Rivers offered shelter from the elements and access to plants and animals for subsistence and ceremonial use, sites for temporary or seasonal camps, places for ceremonial activities and occasionally burial sites.

The Traditional Resources Overview conducted as part of the SEASP preparation, and discussed in Section 2, did not uncover any specific archaeological sites, however, a review of historical records and consultation with Blackfoot Elders confirmed the area known as Six-Mile Coulee to be symbolically meaningful to the Blackfoot peoples and a site of seasonal animal and plant harvesting. Sites such as Six-Mile Coulee and the larger river valley system provided Blackfoot peoples with access to an ecosystem that supported plant and animal life that satisfied subsistence, medicinal and ceremonial needs.

Through consultation with Blackfoot Elders it was determined that the lands adjacent to Six-Mile Coulee should be protected, and that the area should serve a symbolic and educational role as an interpretive park. In so doing, the City of Lethbridge and the Blackfoot peoples will, in partnership, continue the legacy of place-making through storytelling and interpretation.

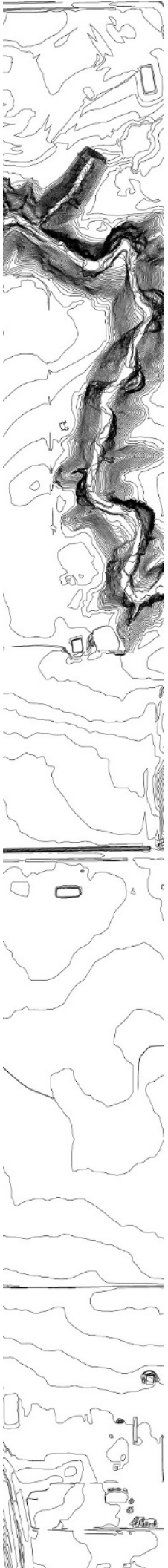
Following the lead of other renowned Blackfoot cultural and interpretive sites in southwestern Alberta, such as Writing-on-Stone (Áísínai'pi), Head-Smashed-In Buffalo Jump and Blackfoot Crossing, the SEASP envisions the Blackfoot Interpretive Park (although at a smaller scale) to be an opportunity for community members of all backgrounds to learn more about the Blackfoot peoples in this region, including language, symbolism, spirituality and land stewardship.

The Blackfoot Interpretive Park will offer an accessible paved pathway along the top of the coulee bank and pathway access into the Coulee. Way finding signage, interpretive signage, art installations, benches and hard and soft landscaping should also be considered along the pathway, including, where possible, at points offering views of the Rocky Mountains, including Chief Mountain. Landscaping shall incorporate, where possible, native plants and plants of traditional use and significance to the Blackfoot.

The design of signage and other public realm elements, such as lighting fixtures, benches, hard landscaping, bicycle stands and play equipment shall be designed and chosen to complement the theme of the Interpretive Park.

Signage and public realm design elements shall be reviewed by Blackfoot Elders to ensure they are historically accurate and culturally appropriate. The design of the Interpretive Park is a unique opportunity for:





- Relationship-building between First Nations and non-First Nations community members.
- Showcasing local Blackfoot artists.
- Creating a welcoming and inclusive public space that showcases the environment.
- Protecting rare and environmentally sensitive areas.

#### 5.2.1 Objectives

- a) Recognize the importance of Six-Mile Coulee in the historical narrative of the Blackfoot people, and their traditional land use of the area.
- b) Create an opportunity for relationship-building and dialogue between First Nations and non-First Nations peoples in Lethbridge.
- c) Provide an opportunity to showcase local First Nations artists in public realm design.
- d) Continue the legacy within the City of Lethbridge of ensuring public access to the river valley system and protecting ecologically sensitive lands.

#### 5.2.2 Policies

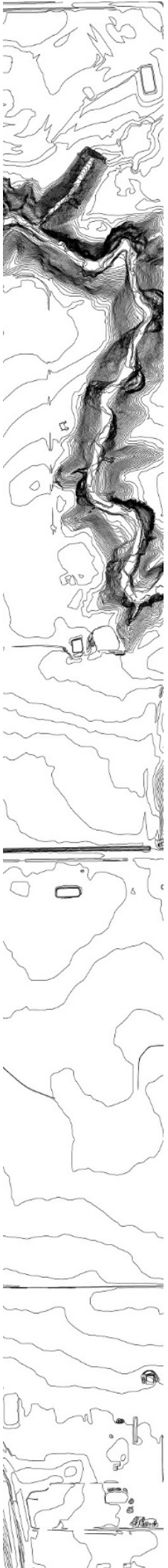
- a) Provide an Interpretive Park adjacent to and above Six-Mile Coulee to ensure public access to Six-Mile Coulee and to protect rare, ecologically sensitive lands.
- b) Subsequent Outline Plans shall determine the extent of the Park and the placement of hard infrastructure to ensure that the installation of such infrastructure does not negatively impact the ecological and spiritual integrity of Six-Mile Coulee.
- c) The Interpretive Park space shall feature a pathway system showcasing local Blackfoot history and culture, such as through interpretive and way-finding signage, public art, demonstration gardens, pedestrian furniture, hard and soft landscaping - and through the culturally appropriate usage of Blackfoot symbolism, storytelling and language.



*Examples of traditional use plants likely found within Six-Mile Coulee and area. From top to bottom: Pasture Sage, Saskatoon Berry, Sweetgrass and Prairie Turnip.*

- d) The design of the Interpretive Park shall be integrated in the adjoining residential areas and shall be undertaken in consultation with Blackfoot Elders.
- e) The Interpretive Park shall integrate with open and park spaces within the plan area and beyond.
- f) The roadway immediately adjacent to the Interpretive Park shall feature a Blackfoot name that reflects the traditional use of the area, such as a plant species or animal.
- g) During the creation of the Interpretive Park, a survey of native vegetation within Six-Mile Coulee shall be conducted and a mitigation plan to reduce the impact on native plant species shall inform the design and maintenance of the Interpretive Park.





# 6.0 Transportation System

## 6.1 Transportation System Principles and Objectives

This section of the SEASP provides objectives and policies with regard to the overall Transportation System within the plan area. Perhaps the most important consideration is the hierarchy of transportation modes, discussed in Figure 11, which emphasizes changing community values in Lethbridge.

In following the City's ICSP/MDP and Transportation Master Plan (TMP), the overall transportation system for the plan area consists of a system of pathways, cycleways and roadways that are efficient and accessible for active and motorized forms of transportation, including pedestrians, bicycles, public transit and private vehicles.

The guiding transportation system principles that were considered during the creation of the SEASP are discussed in Figure 11. These principles point to the need to refocus perspectives of transportation from one that favours private vehicles at the expense of commuters of other modes, pedestrians and recreational users of space, to one that creates an inclusive system of transportation mode opportunities.

The idea that transportation should function as a system suggests that different forms of transportation are both necessary in our current urban paradigm and can be complementary. For example, commuters from the SEASP may want the ability to drive to the central business district during winter months and cycle during the summer. Or, residents may want the ability to walk to a commercial district and return home using public transit.

One way that this can be achieved is by integrating complimentary uses, such as vehicle and bicycle commuters, and separating competing uses, such as commuting cyclists and pedestrians. Combining all uses together poses safety concerns (particularly for mobility challenged users and novice bicycle commuters) and limits the ability of non-motorized commuting. While private vehicle use will be a dominant use of transportation infrastructure

### Transportation System Considerations

**Hierarchy of Transportation Modes** | Following the direction of the ISCP/MDP and the TMP, the hierarchy of transportation modes within the SEASP shall be pedestrians, cyclists and public transit followed by private vehicles. Recognizing that the private vehicles will still be the most dominant mode, the overall transportation system design shall endeavor to balance the continued need for private vehicles with the emerging demand among residents to move through their community and access services without relying on private vehicles.

**Functional** | The Transportation System must be functional. This can be achieved through strong integration to existing arterial and collector roads in surrounding neighbourhoods, and through a transition from a curvilinear road network to a modified grid network.

**Safe & Accessible** | The Transportation System must be safe and accessible. The design of the System shall serve all potential users, including those with mobility limitations, and promote pedestrian and cyclist safety.

**Multi-modal** | A healthy, sustainable community actively encourages multiple modes of transportation, including public transit and bicycles. The design of the internal road network, and the actual roadways and intersections themselves, shall consider how best to promote and equalize access for all forms of transportation, not just the automobile-driving commuter.

Figure 11 Transportation System Considerations

for the foreseeable future, in the SEASP area it will not dominate to the same degree that it has since the middle of the 20th century.

## 6.2 Transportation Integration

The plan area directly connects to the existing neighbourhoods of Fairmont and Southgate. Based on the results of the Transportation Impact Assessment (TIA – see Technical Document Appendix), that was conducted as a technical background document to this Plan, key points of connection are proposed for each of these neighbourhoods to facilitate greater integration and traffic flow for all transportation modes to and from the plan area. Transportation users that benefit from greater integration include pedestrians and cyclists through the pathway and sidewalk system in addition to transit riders and motorists through the road network. This represents a departure from previous planning that has been done for these existing neighbourhoods, but will better connect the plan area to other areas of the City to the north (such as the northeast industrial area) and the west (such as the Mayor Magrath Drive Commercial Corridor). This also has the off-site benefit of reducing potential “short-cutting” by providing more direct and appropriate routes.

Key connections with Fairmont and Southgate are shown on Map 9 and include Fairmont Gate S and both legs of Southgate Boulevard S. In previous planning documents, 43 Street S was intended to extend southward as an arterial roadway with only Fairmont Gate S providing a direct connection to this road. Southgate was not intended to have a direct connection to 43 Street S, as both legs of Southgate Boulevard S were intended to join together when completed.

A total of seven options to connect the road network of the existing Fairmont and Southgate neighbourhoods to the future development in the SEASP area to the east were analyzed by the TIA. It was determined that the road network shown on Map 9 was the best option to provide better transportation connections in Southeast Lethbridge and reduce the potential of “short-cutting” through residential neighbourhoods. With this road network, the TIA projected no significant increase in traffic volume on both legs of Southgate Boulevard when compared to the previously planned road network that offered no direct connection to the east, as found in the Southgate Area Structure Plan and Outline Plan. The projected traffic volumes were still within the standard for collector roadways, which is what Southgate Boulevard has been constructed as.

The TIA also found that the projected traffic volume on Fairmont Gate S was lower, under the road network identified in the SEASP, than what was previously planned in the Fairmont Area Structure Plan and Outline Plan. This previously planned road network had Fairmont Gate S connecting directly to 43 Street S, allowing vehicles from the arterial roadways of 24 Avenue S and 43 Street S to enter Fairmont from the north. Projected traffic volumes for these connection points and other portions of the plan area’s future road network can be found as part of the TIA found in the Technical Documents Appendix.





#### 6.2.1 Objectives

- a) Ensure strong integration between the future SEASP transportation network and the broader existing network in Lethbridge and beyond for all modes of transportation.
- b) Limit traffic impacts on existing neighbourhoods to the west.

#### 6.2.2 Policies

- a) Direct vehicular access points into the plan area from the existing neighbourhoods to the west shall be provided at Fairmont Gate S, the north leg of Southgate Boulevard and the south leg of Southgate Boulevard, to enhance traffic permeability for all transportation modes.
- b) The pathway network in the plan area shall connect with other areas of the City through the regional pathway network and pathway network in adjacent neighbourhoods.

### 6.3 Pathway and Sidewalk Network

Active modes of transportation are promoted within the plan area through the comprehensive pathway and sidewalk network. Pathways and sidewalks will integrate work, play and live spaces to ensure area residents, employees and visitors can transition effectively and comfortably through the plan area. Pathways will also offer direct connections to all major parks and open spaces in the plan area. As described in Section 5, this is a key factor in providing a parks system, as opposed to insular open spaces. Strong active transportation networks, including through pathways, further enhance quality of life within the plan area and encourages healthy living and place-making.

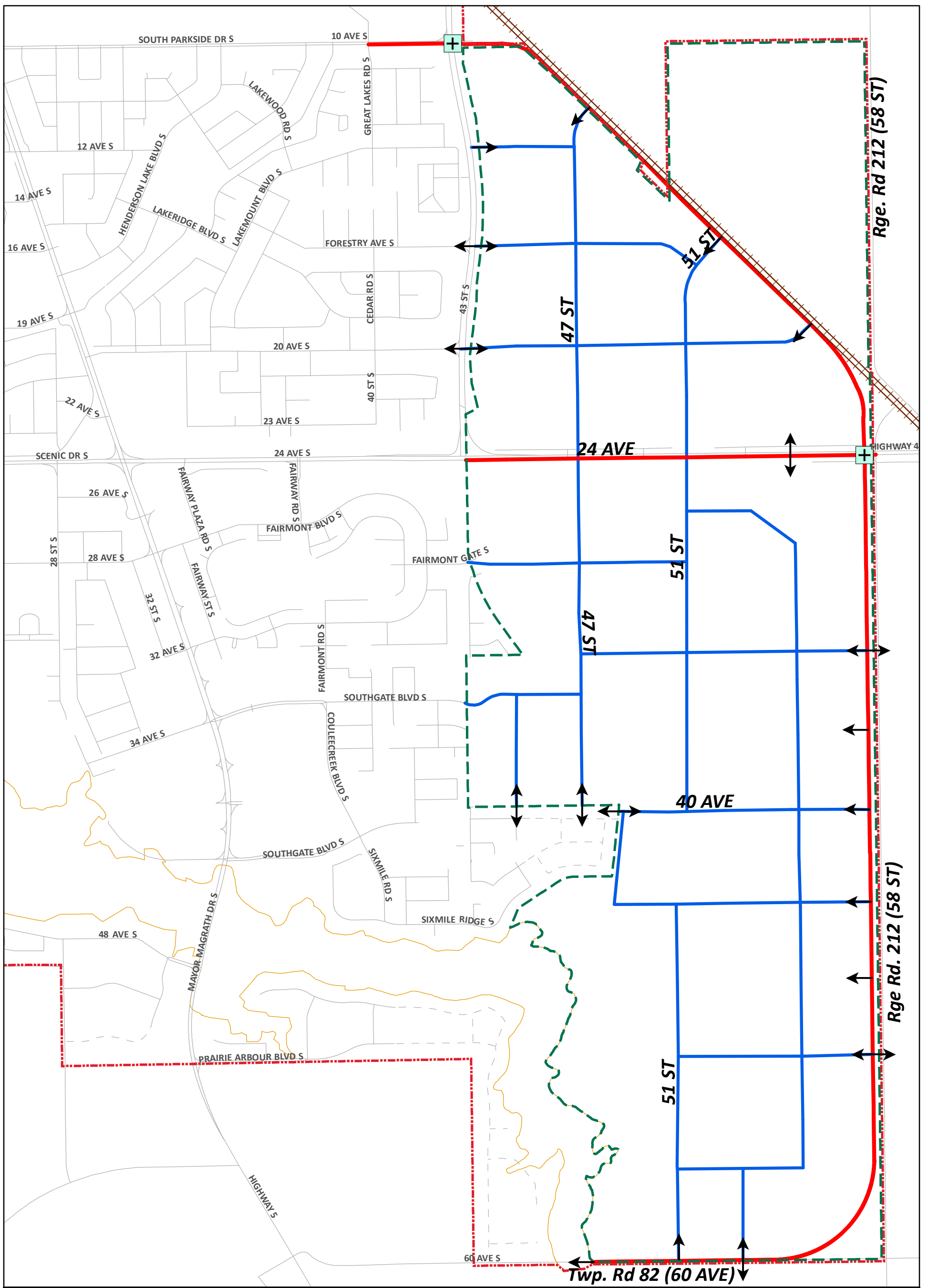
#### 6.3.1 Objectives

- a) Establish a functional pathway and sidewalk network that serves residents, visitors and employees traveling for recreational and commercial purposes and as commuters.
- b) Connect major park nodes and open spaces with the pathway network.
- c) Ensure that pathway and sidewalk user safety and accessibility is paramount throughout the plan area.

#### 6.3.2 Policies

- a) In compliance with City of Lethbridge Transportation Design Standards, a regional pathway shall be located along all arterial roads within the plan area.
- b) The pathway system shall be incorporated into the internal collector road network, where possible. This includes the location of pathways within or adjacent to road right-of-ways.
- c) The pathway network shall connect major park nodes and open spaces, as shown on Map 8.





**Legend**

City Boundary	New/Upgraded Arterial Road
Southeast Area Structure Plan	New/Upgraded Collector Road
Railway	Roadway Connection
Safe Development Setback Line	Required Intersection Upgrade
Existing Roads	
Planned External Roads	

N  
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## MAP 9 - Major Road and Rail Network

The collector and arterial network proposed for the Southeast plan area represents a modified grid system. The intention of the gridded street system is to increase connectivity and efficient travel across the area and integrate this new system with the existing neighbourhoods. Road names within the plan area are preliminary suggestions only.





- d) The pathway network design shall also consider the Objectives and Policies discussed in Section 5.0 Open and Park Space System, including: functionality, safety & accessibility, place-making, and sustainability.
- e) Where feasible, pathways shall be constructed along utility corridors to facilitate maintenance access by utility workers and to enhance open spaces. Specifications for access and load limits for utility access shall be further developed at the design stage in consultation with the relevant utility.
- f) Commercial and institutional land uses shall provide barrier-free pedestrian access between the nearest pathway or sidewalk to the site. Commercial and institutional land uses shall further provide adequately demarcated pedestrian routes through vehicle parking areas to the main and barrier-free entrances of all uses on site.
- g) The exact location of sidewalks and pathways shall be determined at the Outline Plan stage.
- h) Sidewalks shall feature proper direction curb cuts to enhance user safety and accessibility.
- i) Consideration should also be given to raised crosswalks and other pedestrian safety measures at the Outline Plan stage.
- j) Sidewalks will be located along roadways in accordance with City of Lethbridge Design Standards.



## 6.4 Cycling Facilities

When considering the use of cycling, residents can be thought of being allocated into four separate categories. They are the “Strong and Fearless”, “Enthusied and Confident”, “Interested, but Concerned” and “Reluctant to Cycle”. Figure 12 below defines these definitions and provides an estimate of share of the population that each group represents.

This typology was first developed by Roger Geller, Bicycle Coordinator at the City of Portland. It is now commonly employed and has had similar results in other cities, including Lethbridge, which found through the results of a 2016 survey that of the population, 6% were “Strong and Fearless”, 10% were “Enthusied and Confident”, 53% were “Interested but Concerned” and 31% were “Reluctant to Cycle”<sup>3</sup>.

**“Riding a bicycle should not require bravery. Yet all too often, that is the perception among cyclists and non-cyclists alike.”**

*Roger Geller, Bicycle Coordinator, City of Portland*

<10% Strong & Fearless	5-20% Enthusied & Confident	50-60% Interested, but Concerned	30-40% Reluctant to Cycle
			
<ul style="list-style-type: none"> <li>• Most often young &amp; male</li> <li>• Cycling is a part of their identity</li> <li>• Undeterred by road conditions or motor vehicles</li> <li>• Will cycle anywhere, with or without bike infrastructure</li> </ul>	<ul style="list-style-type: none"> <li>• Usually young males &amp; females</li> <li>• Cycle for mainly for recreation and sometimes commuting</li> <li>• Somewhat comfortable sharing the road with motor vehicles</li> <li>• Prefer to use bike infrastructure, such as bike lanes and pathways</li> </ul>	<ul style="list-style-type: none"> <li>• Broad segment of population, including young families and older people</li> <li>• Cycle almost exclusively for recreation</li> <li>• Afraid to ride with motor vehicles</li> <li>• Encouraged to cycle more if bike infrastructure exists.</li> </ul>	<ul style="list-style-type: none"> <li>• Limited exposure to cycling throughout life</li> <li>• No interest in cycling due to topography, inability or lack of interest</li> <li>• Improvements to bike infrastructure unlikely to influence decisions</li> </ul>

Figure 12 The Four Types of Cyclists

<sup>3</sup> Stantec, Bannister Research & Consulting Inc. & City of Lethbridge. February 22, 2016. *2016 Lethbridge Cycling Survey*. [http://www.lethbridge.ca/living-here/Projects-Initiatives/Current-Projects/Documents/cycling\\_mp\\_1\\_project%20background.pdf](http://www.lethbridge.ca/living-here/Projects-Initiatives/Current-Projects/Documents/cycling_mp_1_project%20background.pdf) Lethbridge, Alberta (accessed March 21, 2016).

The two extremes of the population, the “Strong and Fearless” and “Reluctant to Cycle” are usually entrenched in their behaviours and are not likely to change if cycling facilities improve. Therefore, this plan will focus on the “Enthusied and Confident”, “Interested, but Concerned” groups, as they represent a large majority of the population and would benefit most from enhanced bicycle facilities.

To help facilitate better cycling facilities in Lethbridge, the SEASP gives special attention to the incorporation of dedicated and physically separated cycleways along major collector roads within the plan area. Cycleways (also known as cycle tracks or separated bike lanes) separate bicycles from vehicular traffic, increasing the comfort level of bicyclists and improving safety. They will be utilized in the plan area to move bicycle commuter and recreational traffic through the plan area and beyond. Physically separated cycleways will allow bicycle commuters to travel efficiently and safely along the same roadways as private vehicles and public transit, leaving pathways and sidewalks for pedestrians and other users.



Example of a cycleway



Public bicycle repair station

The inclusion of infrastructure, such as public bike repair stations, in the event of a bicycle malfunction or maintenance, and the provision of bike racks will also encourage cycling. Lethbridge currently does not have standards for minimum off-street bicycle parking on private property like it does for vehicles. However, off-street bicycle parking can be easily accommodated through a number of methods such as bike racks, bike lockers or even indoor bike parking. Other municipalities have legislated minimum bicycle parking standards in their land use bylaws. While this is not currently in the Lethbridge Land Use Bylaw, it is intended for a minimum bicycle standard to be used for new commercial and public buildings in the plan area.

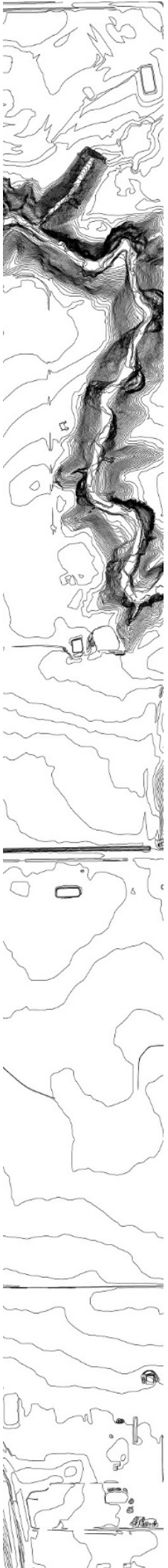
#### 6.4.1 Objectives

- a) Create a network of cycleways that facilitates the movement of users within the plan area and beyond.
- b) Create an environment within the plan area that is convenient, efficient and safe for cyclists and encourages residents to cycle for commuting and recreational purposes.

#### 6.4.2 Policies

- a) Incorporate physically separated cycleways into the roadway design of the Central Parkway and Central Square.





- b) The design of cycleways including lane widths and intersection interface, shall be carried out at the Outline Plan stage.
- c) Subsequent Outline Plans will identify how cycleways will integrate with arterials roadways (including 24 Avenue S and 43 Street S), pathways and the existing road network in neighbourhoods to the west.
- d) Inclusion of cycleways in additional collector roadways, such as the north leg of Southgate Boulevard S will be further considered at the Outline Plan stage.
- e) Commercial and institutional land uses shall provide on-site bicycle parking. This specific requirement shall be determined prior to the Outline Plan stage. This requirement shall identify the minimum number of stalls that are required in addition to other requirements, such as the location of parking stalls on a site or the parking structure that should be used (bike rack etc.). The specific bicycle parking requirement shall be determined based upon an investigation of the bicycle parking requirements for similar land uses in similar cities.

## 6.5 Public Transit

The public transit network will utilize the main collector and arterial roads within the plan area. The modified grid pattern network will increase the efficiency of, and accessibility to, the transit system and help ensure Lethbridge Transit can meet its core service targets. The TIA proposes a number of high level transit routing options, including direct linkages to core employment, institutional and commercial areas within and outside of the plan area (e.g., Lethbridge College and Downtown).

A Transit Master Plan for Lethbridge Transit is also under development at this time, but has not yet been adopted. This Transit Master Plan will further advise the future Outline Plans in the SEASP area.

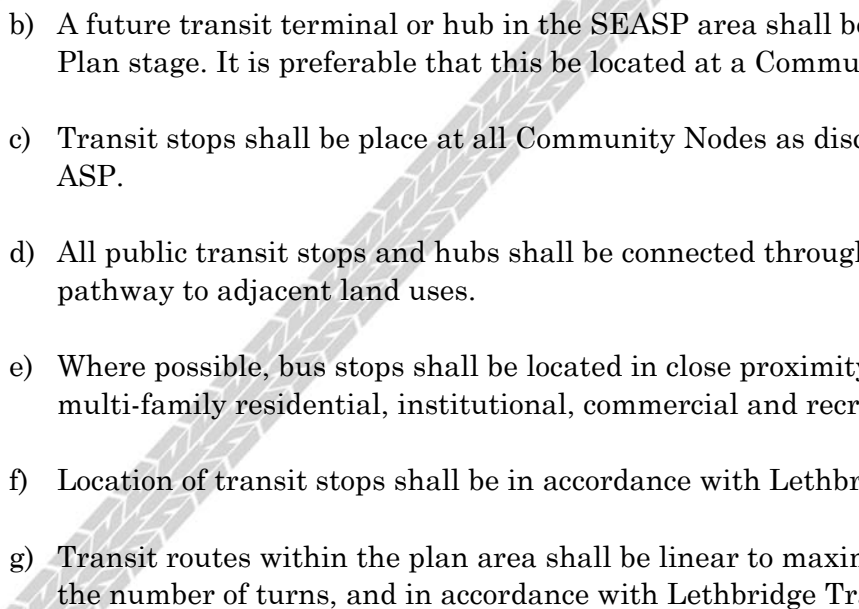
### 6.5.1 Objectives

- a) Provide an adequate level of public transit service.
- b) Encourage public transit ridership to and from residential, institutional, commercial and business/industrial areas by aligning routes with higher potential ridership.

### 6.5.2 Policies

- a) Appropriate transit routes, stops and terminals shall be determined at the Outline Plan stage in partnership with Lethbridge Transit and in accordance with their service standards, and any Transit Master Plan that is adopted in the future at the Outline Plan stage.

**Every Public Transit  
trip starts and ends  
with a user either  
walking or cycling.**

- 
- b) A future transit terminal or hub in the SEASP area shall be considered at the Outline Plan stage. It is preferable that this be located at a Community Node.
  - c) Transit stops shall be placed at all Community Nodes as discussed in Section 4.4 of this ASP.
  - d) All public transit stops and hubs shall be connected through a barrier-free sidewalk or pathway to adjacent land uses.
  - e) Where possible, bus stops shall be located in close proximity to high density areas such as multi-family residential, institutional, commercial and recreational land uses.
  - f) Location of transit stops shall be in accordance with Lethbridge Transit service standards.
  - g) Transit routes within the plan area shall be linear to maximize efficiency by minimizing the number of turns, and in accordance with Lethbridge Transit service standards.

## 6.6 Internal Road Network

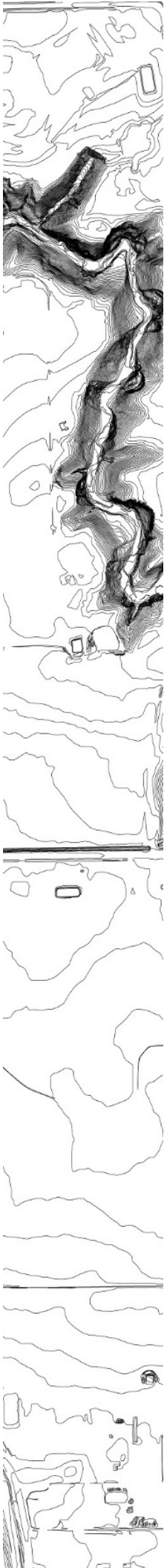
The internal road network for the plan area is to be a modified grid, with transition areas anticipated between the existing curvilinear neighbourhoods to the west. A modified grid pattern road network enhances functionality for all modes of transportation by providing multiple origin-destination routes and therefore greater traffic dispersion, as well as more efficient use of land than a traditional grid road network. The modified grid pattern also supports greater use of public transit as routes can be more direct and pedestrians and cyclists can get to and from bus stops more efficiently.

The plan area will be bound by arterial roads in the south, east, and north as shown on Map 9. Within these bounds, a network of (generally) north-south and east-west collector roads will form the backbone of the internal road network in the plan area by framing the modified grid structure. These collector roadways will occur at various intervals to provide efficient and effective access to all parts of the plan area. In accordance with appropriate transportation design standards, local roads will be considered at the Outline Plan stage.

Where possible, and in consultation with City of Lethbridge Transportation and Emergency Services staff, roads will be designed to be the minimum possible width, with the minimum number of lanes, that can safely, effectively and efficiently allow for the desired movement of pedestrians, cyclists, public transit and private vehicles. Narrower roadways increase pedestrian safety by reducing crossing distances and encourage safe driving behavior. Narrower roadways also result in decreased capital and maintenance costs and increase the amount of land for other transportation modes, open and park space and private development.

Road widths may be reduced in a number of ways, including incorporating crosswalk bump-outs with on-street parking, incorporating dedicated cycleways in lieu of private vehicle





parking, incorporating restricted use streets (e.g. streets restricted to local traffic and cyclists, or restricted to public transit), or limiting on-street parking to one side of the roadway rather than both.

#### 6.6.1 Objectives

- a) Develop an internal road network that promotes the orderly and efficient use of land.
- b) Ensure the internal roadway network is functional through strong integration with existing neighbourhoods.
- c) Ensure the SEASP area internal road network is connected to the arterial roadways and is safe and accessible.
- d) Ensure that the internal roadway network accommodates all modes of transportation.
- e) Use space efficiently by reducing road widths, where possible.

#### 6.6.2 Policies

- a) The internal road network shall integrate with existing arterial and collector roadways to the west of the plan area, as well as 24 Avenue S, where feasible.
- b) The internal road network shall be designed as a modified grid pattern. Where required, transition areas from the existing curvilinear network in established neighbourhoods in the west to the modified grid network in the plan area will be considered.
- c) The alignment of all local roadways will be determined at the Outline Plan stage.
- d) Roadway and intersection design will consider active transportation and pedestrian accessibility.
- e) Roadways shall be designed to be the minimum possible width to promote safety and efficiency of all transportation modes (in line with the Transportation System Considerations).

### 6.7 Arterial Roadways

The SEASP area contains or is adjacent to three arterial roadways that will directly serve the plan area: 43 Street S, Highway 4 (24 Avenue S) and 58 Street S / 60 Avenue S. These roads all exist, but will need to be upgraded or expanded as the area develops.

Highway 4 (24 Avenue S) will remain in its current alignment, with existing intersections upgraded and new intersections built as required. This segment of Highway 4 within the City has a speed limit that varies between 100 km/hr and 80 km/hr and will be lowered to an appropriate level in advance of intersections being built or upgraded as the surrounding area undergoes urban development. Highway 4 (24 Avenue S) is considered a “gateway corridor”



into the City and will utilize extensive landscaping and aesthetic features. Design requirements for this corridor are further discussed in Section 6.8 of this ASP. 43 Street S will remain an arterial road in its current alignment north of 24 Avenue S, however, the portion of 43 Street S, south of Highway 4 (24 Avenue S) will terminate here and will not run continuously through the plan area. 58 Street S will be extended north of Highway 4 (24 Avenue S) and run parallel to the railway. This arterial roadway will intersect with 43 Street S and South Parkside Drive S.

The existing right of way for 58 Street S (Range Road 212) is adjacent to the City boundary, but is within Lethbridge County. As the SEASP area is developed, a new right of way for 58 Street S will be established immediately to the west of the current rural road right-of-way, so that it is located inside the City boundary. The existing 58 Street S (Range Road 212) right of way contains a gravel road within the County and serves adjacent existing residences. Preserving this gravel road is the responsibility of Lethbridge County and, although these existing residences cannot access the future arterial roadway directly, provision has been made to allow this existing gravel road access to the future road network in the SEASP area at select locations. In the future, the existing 58 Street S (Range Road 212) rural road right of way and the future 58 Street S / 60 Avenue S arterial roadway may both exist parallel to one another. To avoid having two intersections in close proximity, they will be consolidated into one intersection at 24 Avenue S. The design of this singular intersection will be completed prior to the construction of the 58 Street S / 60 Avenue S arterial roadway with input from The City of Lethbridge, Lethbridge County and Alberta Transportation.

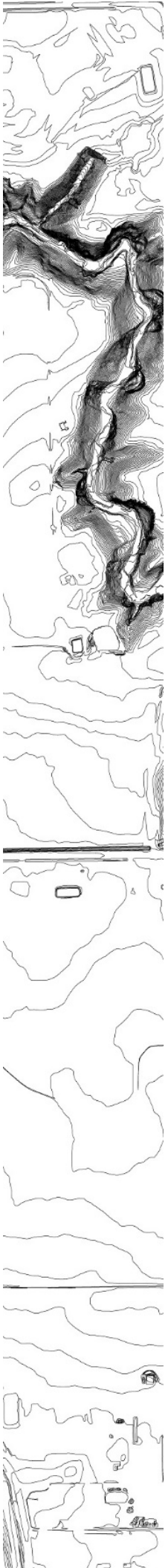
The 58 Street S / 60 Avenue S arterial roadway will turn west in the southeast corner of the SEASP boundary and follow the 60 Avenue S alignment, where it will eventually connect to Highway 5 near the Lethbridge County Airport.

Two Lethbridge County roads, Township Road 82 and Range Road 212, currently have direct access to the City via 60 Avenue S and 58 Street S, where they intersect at the southeast corner of the plan area. Due to safety issues and the need for arterial roadways to have limited access, this existing intersection will need to be removed when the future 58 Street S / 60 Avenue S arterial is established. From this arterial roadway access to Township Road 82 will then be provided at an intersection further west and access to Range Road 212 will then be provided at an intersection further north as shown on Map 9.

The standard right-of-way width for an arterial roadway in Lethbridge is currently 75 metres, which includes 15 metres on either side for landscape buffering to shelter residential dwellings from undue noise and visual impacts. However, it is recommended that this right-of-way width be narrowed within the SEASP by removing the 15 metre buffer wherever the roadway is not adjacent to residential development. Commercial and industrial development would benefit from increased visibility on a major road.

Access to the arterial roadways will be spaced at adequate intervals in accordance with City of Lethbridge guidelines.





#### 6.7.1 Objectives

- a) Ensure adequate land is provided for the development of arterial roadways that will serve the SEASP area.
- b) Ensure that access to arterial roadways is both safe and efficient.

#### 6.7.2 Policies

- a) The portion of the existing 43 Street S right of way that is located south of 24 Avenue S will be closed to vehicular traffic when an alternative access to Fairmont Gate is provided.
- b) As the area north of 24 Avenue S is developed, a right of way for a new arterial roadway that runs parallel to the railway, the existing 58 Street S and 60 Avenue S shall be developed inside of the City boundary.
- c) The design of a singular intersection that consolidates the access to 24 Avenue S (Highway 4) from the future 58 Street S arterial roadway and adjacent the adjacent 58 Street S rural road right of way will be completed prior to the construction of the 58 Street S / 60 Avenue S arterial roadway with input from The City of Lethbridge, Lethbridge County and Alberta Transportation.
- d) Access to the future SEASP area road network from the existing 58 Street S (Range Road 212) , located in Lethbridge County can be provided at appropriate intervals and in consultation with the City of Lethbridge Transportation department.
- e) Access to the future 58 Street S / 60 Avenue S arterial roadway from Range Road 212, and Township Road 82 shall be made at the safest and best possible locations. Future intersections with the future 58 Street S / 60 Avenue S arterial roadway shall not be made on a curve.
- f) Arterial roadway right-of-ways shall be narrower than the current City of Lethbridge standard of 75 m, wherever extra space within the right-of-way is not required for utility purposes or the buffering of adjacent residential uses. Final determination of the right-of-way width shall come at the Outline Plan stage.
- g) All-directions access to the arterial roadways will be permitted at intervals of approximately 400 metres, and right-in/right-out access shall be permitted at intervals no closer than 200 metres in accordance with City of Lethbridge design standards. Variations from this standard are subject to the approval of City of Lethbridge Transportation department.
- h) The ultimate development of 24 Avenue S shall be subject to the 24 Avenue S Gateway Corridor guidelines that are found in Section 5.0 of this document. These guidelines tie together the technical considerations of the arterial roadway with the adjacent land uses.

## 6.8 24 Avenue Gateway Corridor

The segment of 24 Avenue S (Highway 4) within the SEASP area is the entrance for Highway 4 into the City and is intended to be a “gateway” into Lethbridge. This concept is especially important, as Highway 4 carries a great deal of both international and regional travelers into the city. This is defined in the City of Lethbridge and County of Lethbridge IDP, as *“Highway 4 and 5 in the south, should be given special consideration by both municipalities for approvals to protect and enhance the view with special landscaping, signage or other features”* (IDP Policy 5.4.13).

Establishing a sense of “gateway” involves creating a strong first impression and identity that relates to the City of Lethbridge, as well as responding to the aesthetic and functional requirements of the corridor. References to Lethbridge’s heritage, cultural and historic landscapes and architectural styles, using common colours and materials can serve as inspiration for themes and character to be incorporated into the gateway and its feature elements. Examples of such materials can include natural stone, tinted concrete and brick and metal cross-members similar to those used on the High-Level Bridge.

It is important to note that the 24 Avenue Gateway Corridor must continue to function as a major roadway that connects Highway 4 to Highways 3 and 5 and will continue to convey high volumes of both commercial and personal traffic.

The following is a description of the proposed features and overall aims for the Gateway Corridor:

**Multi-use Pathway.** A multi-use pathway for pedestrians and cyclists will be constructed between the existing ditch and proposed commercial and residential development on the south side of the 24 Avenue S right-of-way. This pathway can also utilize pedestrian scale lighting that minimize light pollution and provide a safe environment.

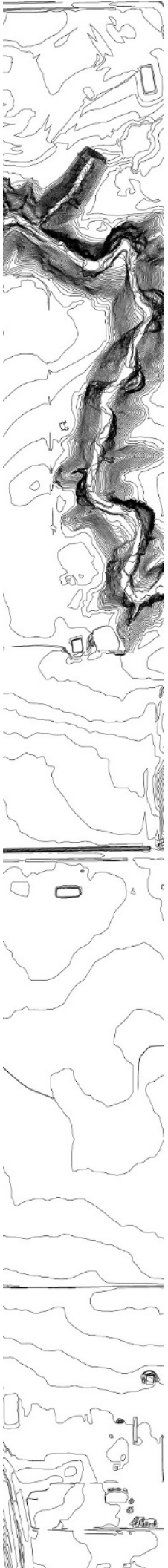
Connecting this pathway to the existing regional pathway network and the future pathway network that is internal to the SEASP area will encourage the use of non-vehicular modes of transportation between neighbourhoods and new amenities within the SEASP area.

Similar multi-use pathways have been used in Lethbridge for a number of years, with great success.



*Existing multi-use pathway parallel to Mayor Maqrath Drive*





**Landscaping.** Extensive landscaping will be used in the 24 Avenue Gateway Corridor, however care must be taken to ensure that vegetation does not unnecessarily encroach into the roadway or block sightlines. A small landscaped berm, planted with trees and shrubs will be constructed between the multi-use pathway and adjacent residential development on the south side of 24 Avenue S to provide a privacy buffer. Tree and shrub planting will occur adjacent to commercial and business industrial development located on the north side of 24 Avenue S to help screen parking areas, loading docks and utility services, and to break up large expanses of concrete. Vegetation, in the form of shrubs, trees and/or ornamental grasses will be planted in the centre median of 24 Avenue S.



*Existing boulevard and median landscaping on Mayor Magrath Drive.*

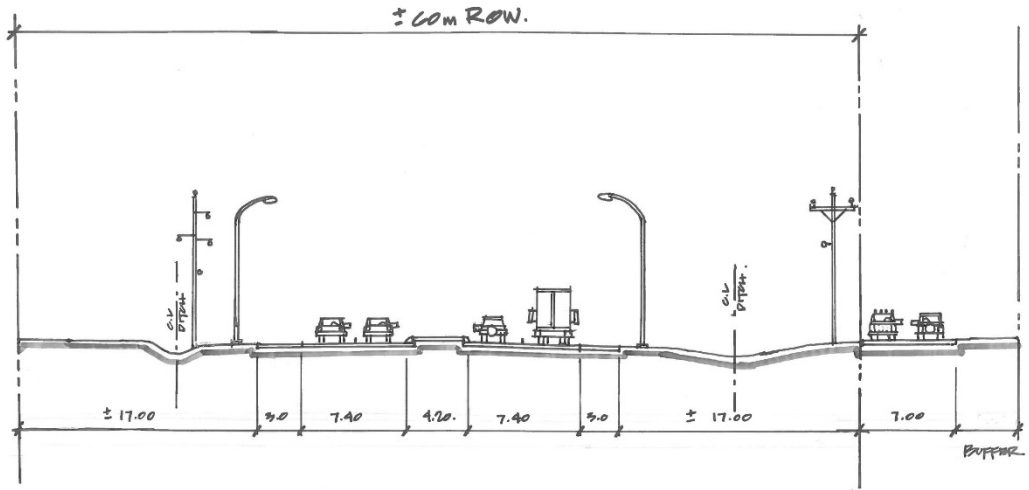
Landscaping can also take the form of xeriscaping, which uses drought resistant and native plant species, and typically requires less maintenance than more traditional manicured and irrigated areas.

**Signage and Street Furniture.** Signage and street furniture help to create an identity and sense of place and will be used in the Gateway Corridor. Decorative streetlight fixtures, banner art and pageantry that are complementary to proposed corridor enhancements will be utilized on the boulevard and/or centre median. Wayfinding signage for both vehicles and pedestrians will be provided at close proximity to intersections for orientation and directions to key destinations. Vehicular and pedestrian crossings of the existing ditches on the north side of 24 Avenue S can use false bridge elements to appear as though they are small bridges.

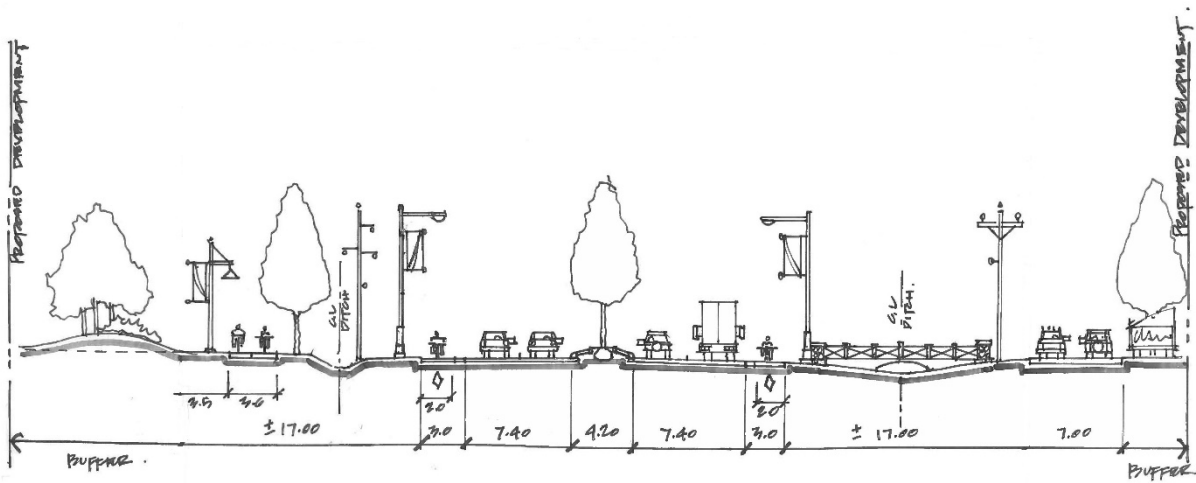


*Examples of themed wayfinding signage*

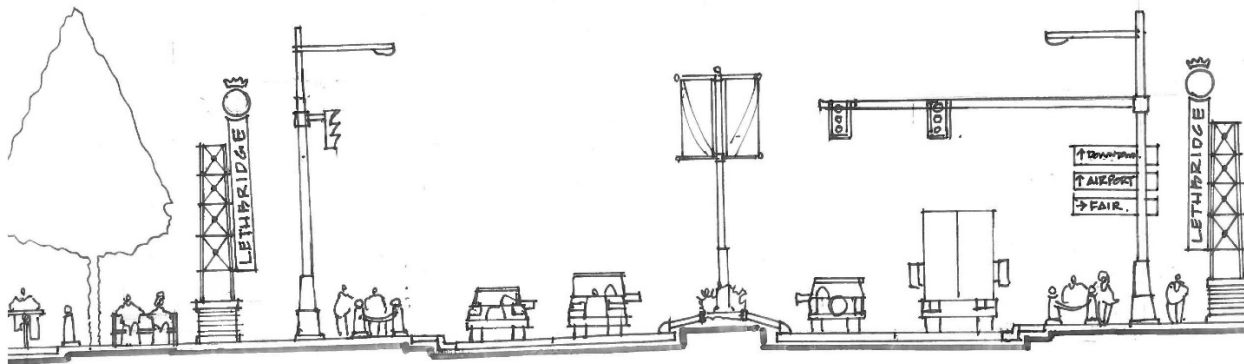
New billboards, other than those that already exist adjacent to 24 Avenue S, will not be permitted in the Gateway Corridor.



The 24 Avenue right-of-way as it currently exists

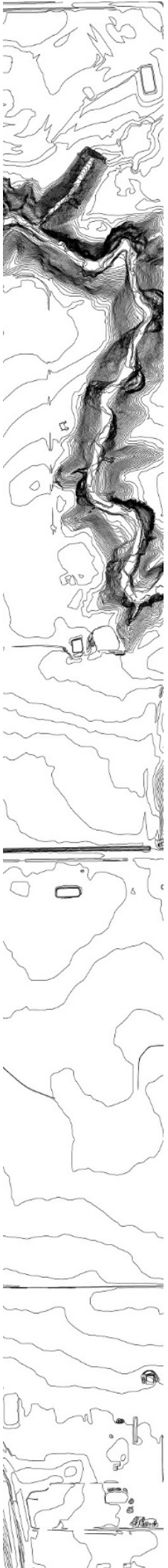


A conceptual image of the potential, future 24 Avenue right-of-way



A conceptual image of the potential, future intersection of 24 Avenue S and 43 Street S





Specific design requirements will be determined in the future as the surrounding area is developed and the necessary upgrades to 24 Avenue S are made. This will allow the technical roadway design of the area to also consider the goals that are outlined in this ASP and the relationship between public lands and adjacent uses. This will also ensure that the latest technologies can be utilized and that actual cost implications are better understood at the time imminent to construction.

The following criteria will be considered in the design requirements for the 24 Avenue Gateway Corridor:

- All-Season Use of the Public Lands
- Multi-Modal Transportation Functionality
- Appropriateness Considering the Local Climate
- Crime Prevention Through Environmental Design
- Aesthetic Design Principles
- Minimal Impacts Upon the Natural Environment
- Cost

#### 6.8.1 Objectives

- a) Create a “gateway” into the City that conveys a strong first impression and identity that relates to the City of Lethbridge.
- b) Create an area that effectively and safely accommodates multiple modes of transportation.
- c) Provide guidance for the appearance of the 24 Avenue Gateway corridor at full development.

#### 6.8.2 Policies

- a) A multi-use pathway that connects to the existing regional pathway network and the future pathway network that is internal SEASP area shall be constructed on the south side of 24 Avenue S.
- b) The multi-use pathway shall have street furniture, such as pedestrian-scale lighting, benches and waste/recycling receptacles to encourage their use by pedestrians and cyclists for recreational, as well as commuting purposes.
- c) Medians and boulevards that are contained with the 24 Avenue S right-of-way shall contain landscaping and vegetation consisting of trees, shrubs and ornamental grasses.
- d) The use of decorative streetlight fixtures, banner art, pageantry and wayfinding signage that are complementary to proposed corridor enhancements shall be on the boulevard and/or centre median of 24 Avenue S.

- e) The use of false-bridge elements shall be considered for vehicular and pedestrian crossings of the existing ditch on the north side of 24 Avenue S.
- f) New Billboards, other than those that already exist adjacent to 24 Avenue, shall not be permitted in the 24 Avenue Gateway Corridor.
- g) Requirements for the specific aesthetic design and functionality of the 24 Avenue Gateway Corridor, as described in section 6.8 of this document, shall be further determined in conjunction with future roadway upgrades to 24 Avenue S. Factors such as the all-season use of the public lands, multi-modal transportation functionality, appropriateness considering the local climate, crime prevention through environmental design, aesthetic design principles, minimal impact upon the natural environment and cost must also be considered in the development of these requirements.

## 6.9 Railway Infrastructure

Due to the inability to construct sufficient spur lines, the CPR rail line located within the plan area is not feasible for use by future nearby commercial or business industrial uses due to its short length. Future commercial and business industrial uses may, however, be served through other rail yards, such as the facilities found within Lethbridge County approximately 12 kilometers south of the plan area, the rail yard in Kipp and the City's rail loading facility in North Lethbridge.

Recommended setbacks from rail facilities vary depending upon the type of land use that is developed near these facilities. The Railway Association of Canada and the Federation of Canadian Municipalities have published recommended setbacks from rail facilities in the document, *Guidelines for New Development in Proximity to Railway Operations*. Required development setbacks from the railway will be identified at the Outline Plan stage and these setbacks will be determined using this document and in consultation with the railway operator.

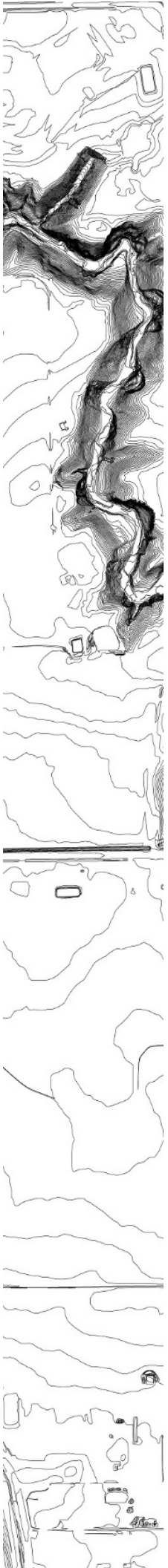
### 6.9.1 Objectives

- a) Encourage land uses located in the SEASP area to access rail services located outside of the plan area.

### 6.9.2 Policies

- a) The railway that runs through the SEASP area will not be accessed directly from uses in the SEASP area.
- b) Partnerships to for land uses in the SEASP area to access multi-modal rail facilities in Lethbridge County and elsewhere in the City are encouraged.
- c) Development setbacks between the existing railway and new development will be identified at the Outline Plan stage. These setbacks will be determined using the





document, *Guidelines for New Development in Proximity to Railway Operations* and in consultation with the railway operator.



# 7.0 Utility Servicing

This Section of the SEASP provides objectives and policies with regard to the provision of utility services in the plan area. The information and policies contained in this Section originate in the South East Area Structure Plan Utility Servicing Plan document that is included as part of the Technical Documents Appendix. The Utility Servicing Plan addresses the provision of stormwater, sanitary sewer and water distribution services to developments within the plan area.

For the purposes of preparing the Utility Servicing Plan, the SEASP area is divided into five servicing areas (some containing sub-areas). Where required, sub-areas are identified for each utility servicing concept and are discussed below. Map 10 indicates the servicing areas.

## 7.1 General Information

The servicing concept for the SEASP area is based on the natural topography and drainage of the area, as well as connection points to existing infrastructure. The plan area is divided into general servicing or catchment areas that are created by the area's topography and natural drainage and then adjusted to match road and infrastructure location. The subsequent sections describe utility servicing strategies for individual catchment areas.

## 7.2 Stormwater Management System

The stormwater servicing concept is shown on Map 11. In total, seven stormwater catchment areas are identified. The concept identifies the ultimate need to construct a new stormwater outfall along the base of Six-Mile Coulee to the Oldman River. The exact location of the outfall along Six-Mile Coulee will be determined at subsequent stages of planning in the southeast.

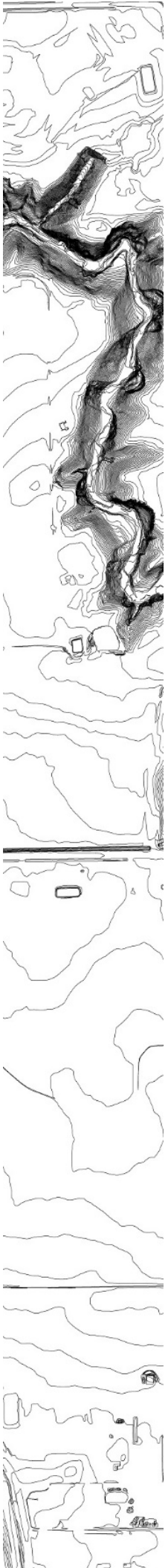
Table 5 Stormwater Catchment Areas

Stormwater Catchment Areas	Size (ha)
A	174
B1	103
B2	149
C	169
D	76
E	8

The storm sewer outfall in Six-Mile Coulee will be sized to accommodate the development of the plan area, and a pipeline diameter of 1200mm will be required. The pipe size will be increased to tie in existing outfalls draining into the coulee from the SMRID return flow, and from development in Prairie Arbour Estates, Southgate, Mayor Magrath Drive S, Southridge and Sandstone Ridge. This will reduce surface flow along the coulee bottom, and thereby help minimize further erosion of the coulee.

Each catchment area will be serviced by a standard urban stormwater collection system consisting of catchbasins, storm sewers and stormwater management facilities connected to the new outfall. The post-development release rate for all catchments will be 3.1 L/s/ha and 1100 m<sup>3</sup>/ha of active stormwater storage is required. A minimum of 1 storm pond facility will be required for every 35ha of development. At least one wet pond facility will be provided for each connection to the outfall.





The very low release rate of 3.1 l/s/ha means that dry ponds will need to be designed with sufficient volume to attenuate flows to a point where they can be conveyed by pipe to downstream wet ponds.

**Area A.** Stormwater management facilities will be constructed in the vicinity of natural low points adjacent to the CPR line. All ponds will connect to a single stormwater pump station located immediately east of the SMRID Canal that will pump into the outfall trunk, terminating just north of Highway 4 (24 Avenue S) and west of the canal. Emergency overflow of this system will spill to the existing natural drainage course northeast across the CPR line.

**Area B.** The natural drainage in this area is split into two catchments (B1 and B2) by a ridge running roughly west to east midway through servicing Area B.

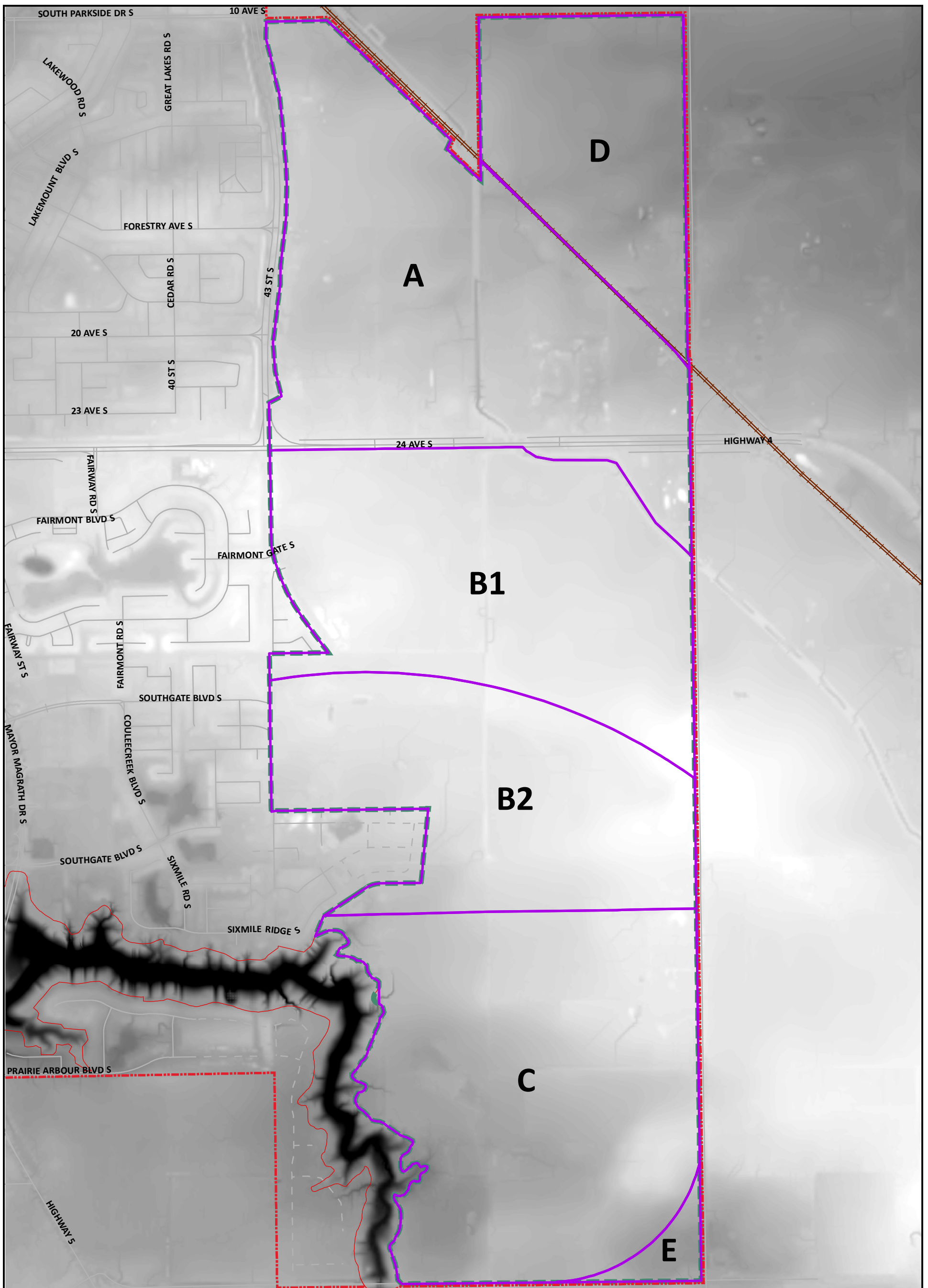
Storm service to Area B will be through a gravity storm sewer trunk connected to the outfall in Six-Mile Coulee. The storm sewer trunk will be installed in a utility right of way, either on its own or with other infrastructure. The pipeline will terminate just north of Highway 4 and west of the canal. Crossing through the ridge, the pipe will reach depths of 8 to 10m. A utility right of way of approximately 25 metres, housing the high pressure gas pipeline, the Sanitary Sewer Trunk and the storm trunk will be required where this trunk follows this gas pipeline, which is much of the required distance. Where this storm trunk runs parallel to the Central Parkway, it will be located on the east side of this road.

**Area C.** Future servicing of this area will be accomplished by a connection to the Six-Mile Coulee outfall. The trunk for Area C will connect to the outfall near its intersection with the ATCO high-pressure gas line. The storm system will be extended along roadways in the area as development proceeds and as detailed at the Outline Plan stage.








Alberta Transportation and Lethbridge County have also planned to construct a drainage channel that will help to drain stormwater from Lethbridge County along 60 Avenue S to Six Mile Coulee. This drainage channel will be incorporated into the future stormwater management system in Area C.

**Area D.** Servicing this land with traditional urban storm water systems is difficult because it is significantly lower than areas south of the CPR line. The railway line also blocks efforts to move stormwater to the south and west. In addition, the existing systems to which these lands drain are currently subject to overland flooding. As a result this area cannot support development that would increase either stormwater runoff rates or volumes.

**Area E.** This area has no natural overland outlet. The area is too low to be easily serviced with a stormwater trunk line making it unsuitable to be developed as a stormwater retention facility and, with no overland outlet, it is naturally subject to periodic flooding. Development



**Legend**

-  City Boundary
-  Southeast Area Structure Plan
-  Safe Development Setback Line
-  Railway
-  Existing Roads
-  Planned External Roads
-  Servicing Area

**Elevation**


High : 925 m

Low : 879 m

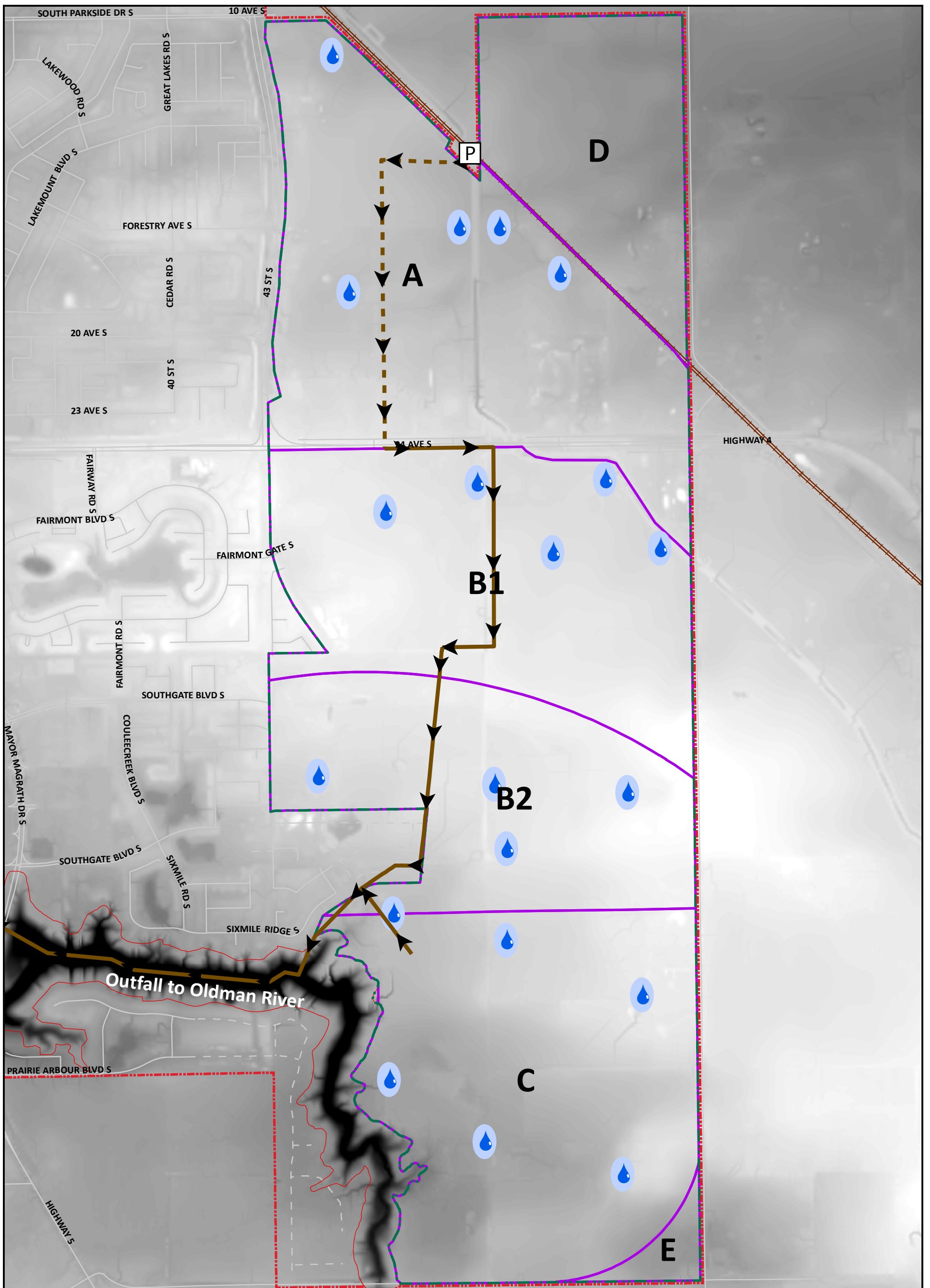
0 125 250 500 Meters

## MAP 10 - Servicing Areas

There are five main servicing areas within the Southeast plan area reflecting the major areas for sanitary, storm, and water servicing defined by topographic features, such as ridges and natural depressions, and physical constraints, such as major roadways and the rail line. Main areas A and B reflect the complications of various delineations splitting up the servicing area, and are thus subdivided into sub-areas for more detailed description and analysis.







**Legend**

- City Boundary
- Southeast Area Structure Plan
- Railway
- Safe Development Setback Line
- Existing Roads
- Planned External Roads
- Stormwater Facility
- Servicing Area
- Pump Station
- Gravity line
- Forcemain

**Elevation**  
 High : 925 m  
 Low : 879 m

0 125 250 500 Meters

## MAP 11 - Stormwater Management

The stormwater servicing concept for the Southeast will ultimately consist of a new storm sewer outfall built along the bottom of Six Mile Coulee going to the Oldman River. Each sub-catchment area will be serviced by a standard stormwater collection system consisting of catch basins, storm sewers, and stormwater management facilities connected to the new outfall.



adjacent to this area should be constructed so that it drains away from this low area, leaving this area unchanged from a storm water perspective.

#### 7.2.1 Catchment B1, and B2 Interim Strategies

Based on the development sequencing discussed in Section 8 of this document, indications are that development within the plan area will begin along the west edge of storm catchment Area B. Financial constraints mean that the installation of the ultimate storm sewer outfall in Six-Mile Coulee must be delayed as far into the future as possible. As a result, interim strategies for Areas B1 and B2 adjacent to existing development in Fairmont and Southgate are required.

The initial stages of Area B1 will be serviced through the Fairmont neighbourhood via the development of seasonal storage facilities. These facilities will be larger than what will ultimately be required and will need to be designed to allow them to be redeveloped at a future point into the lake amenities connecting to the ultimate stormwater outfall. These first phases of development in Area B1 will eventually connect into the existing system in Southgate through the existing Mayor Magrath Drive S outfall, utilizing a maximum of 100 L/s of the 200L/s available from that system. This will allow 32.25 ha of land to be developed and serviced in Area B1 before the future outfall in Six-Mile Coulee is required. The specific flow rate and land area that can be developed using existing Southgate capacity in catchment B1 will be finalized during the outline planning process.

Development in the initial stages of Area B2 will begin in the vicinity of Southgate Boulevard S at the east edge of Southgate. This land will connect through the existing Southgate system and utilize 100 L/s of the existing capacity available in the Mayor Magrath Drive S outfall, allowing 32.25 ha of land in Area B2 to have stormwater servicing before the future outfall in Six-Mile Coulee is required. As development within B2 progresses it will be required to extend trunk sewer connections northward providing capacity of 100 L/s for development within Catchment B1. The specific flow rate and land area that can be developed using existing capacity in catchment B2 will be finalized during the outline planning process.

#### 7.2.2 Objectives

- a) Ensure that the impact of development on preexisting stormwater runoff is minimized.
- b) Provide a stormwater management system that is efficient, effective and financially viable to construct and maintain.
- c) Acknowledge the challenges inherent in servicing low lying land discourage development where servicing is not efficient, effective and financially viable.
- d) Protect environmentally significant features on the landscape, including wetlands, through careful planning and the design of Stormwater Management Facilities.





### 7.2.3 Policies

- a) The stormwater servicing strategy shall ultimately consist of a new storm sewer outfall at Six-Mile Coulee.
- b) Stormwater management facilities shall be designed to provide sufficient storage volume.
- c) Final determination of the specific location and number of stormwater management facilities shall be made at the Outline Plan stage, with the approval of the City of Lethbridge Infrastructure Department.
- d) The stormwater management system shall meet the standards and best practices that exist at the time of development.
- e) The drainage channel that has been planned by Alberta Transportation and Lethbridge County to run parallel to 60 Avenue S will be incorporated into the future stormwater management system in Area C.
- f) Where feasible, stormwater management facilities shall be incorporated into the green and open space areas providing an amenity for residents, workers and visitors to the plan area.
- g) Stormwater management facilities shall consist of both wet and dry ponds. Determination of whether a facility is either a wet or dry pond shall be made in context of the surrounding land uses at the Outline Plan stage and whether irrigation water can be used as make-up water to fill a given storm pond. For example, a storm pond in a residential area might be a wet pond that is also used as an amenity, where a dry pond may be adequate in an industrial setting.
- h) All stormwater must be treated in a stormwater pond facility prior to being discharged into the Oldman River through a stormwater outfall.
- i) A new storm sewer outfall trunk shall be constructed at the bottom of Six-Mile Coulee. This outfall trunk shall have a minimum pipeline diameter of 1200mm to convey stormwater from the plan area and other nearby existing neighbourhoods to the Oldman River system.
- j) The design of the new Six-Mile Coulee outfall shall require necessary environmental and regulatory approvals prior to construction.
- k) At the time when detailed studies are conducted to design the Six-Mile Coulee outfall, consideration shall be given to undertaking additional traditional land use studies, including a survey of native plants within Six-Mile Coulee.



## 7.3 Sanitary Sewer Collection System

Internally, the plan area will be serviced by a standard urban sanitary sewer system following the alignment of collector and circulation roadways. Areas A and B1/B2 can be serviced by gravity. Area C is located on the other side of a ridge from Areas A and B1/B2 and will require that the wastewater it generates be pumped through a forcemain to the other side of this ridge. For the development of Area C to commence, a lift station will need to be constructed in the southwest of the plan area prior to development in this area, as shown on Map 12. The exact location of the lift station required for Area C will be determined at the Outline Plan stage. Areas D and E are significantly lower in elevation than the rest of the development area. Servicing these areas is not considered in the SEASP because adequate levels of service cannot be efficiently provided for sanitary sewer services. The cost of operating and maintaining the sanitary sewer infrastructure for Areas D and E makes them unfeasible from an operational standpoint.

The sizing and alignment of the collection system will be refined at the Outline Plan stage. It is recommended that Outline Plans include Environmental Protection Agency Storm Water Management Modeling (EPASWMM) of the distribution system for the Outline Plan area using the wastewater generation rates shown in the Utility Servicing Plan. The Utility Servicing Plan in the Technical Documents Appendix provides wastewater generation rates for each sewershed based on proposed land uses.

Table 6 Sanitary Sewershed Areas

Sanitary Sewershed Areas	Size (ha)
A	75.5
B1 / B2	133.5
C	82.5
D	0
E	0
<b>Total</b>	<b>291.5</b>

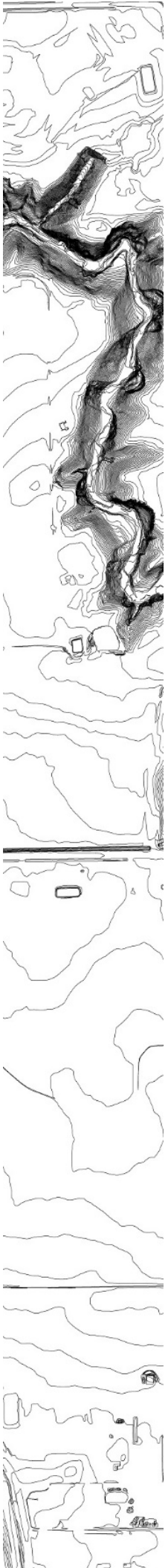
### 7.3.1 Existing Available Off-Site Capacity

There is enough residual capacity available in the existing off-site sanitary sewer infrastructure in South Lethbridge to service a small area of approximately 25 hectares located adjacent to the existing developed segment of Southgate. This will allow this portion of the SEASP to undergo development within the near term future, subject to the adoption of an Outline Plan and all necessary approvals, as this area can make use of existing infrastructure. At the time of writing, neither the existing Mayor Magrath Drive S nor the 2 Avenue N lift stations have any residual capacity to service new development in the SEASP area, beyond these initial 25 hectares. Servicing the remainder of the SEASP area will require additional capacity to be identified and created through upgrades to the existing system or the construction of the ultimate servicing connections. No upstream connections are anticipated or can be considered because of the existing downstream capacity constraints. Therefore, beyond the initial 25 hectares, development of the remainder of the SEASP will be delayed until one of the off-site sanitary sewer servicing options is constructed, as described under section 7.3.2 of this document.

### 7.3.2 Ultimate Off-Site Servicing Strategy

Offsite sanitary servicing, which will convey wastewater to the City's wastewater treatment plant in North Lethbridge, will consist of one of two potential sewer system route options. They are summarized as follows:





- a) **10 Avenue Regional Lift Station Route** –A regional lift station located in the vicinity of 10 Avenue S and 43 Street S would be constructed to pump wastewater northward from the SEASP area through an existing forcemain located in a railway right of way, outside of the plan area, to an existing lift station located at 2 Avenue N. From here a combination of existing and new infrastructure would be used to convey wastewater to the City’s wastewater treatment plant.
- b) **Scenic Drive Route** - A gravity fed sewer would be constructed heading west from the plan area at the intersection of 24 Avenue and 43 Street, along Scenic Drive S / 24 Avenue S and into the river valley. Once in the river valley, it would tie in to an existing sanitary sewer vault located just south of Whoop Up Drive. From this point wastewater would be conveyed to the City’s wastewater treatment plant through existing infrastructure.

These options are fully described in the servicing reports, found in the Technical Documents Appendix and are shown on Map 13 of the ASP document. Both options require a substantial amount of new sanitary sewer pipeline to be constructed, have similar overall costs and staging requirements and have their own particular advantages. In general, the internal servicing of the plan area is the same for both options, with the following exception: With the 10 Avenue Regional Lift Station Route, the sanitary sewer north of 24 Avenue S will drain northwards towards the potential regional lift station. With the Scenic Drive Route, the sanitary sewer north of 24 Avenue S would instead drain southwards towards the potential off-site connection near the intersection of 24 Avenue S and 43 Street S.

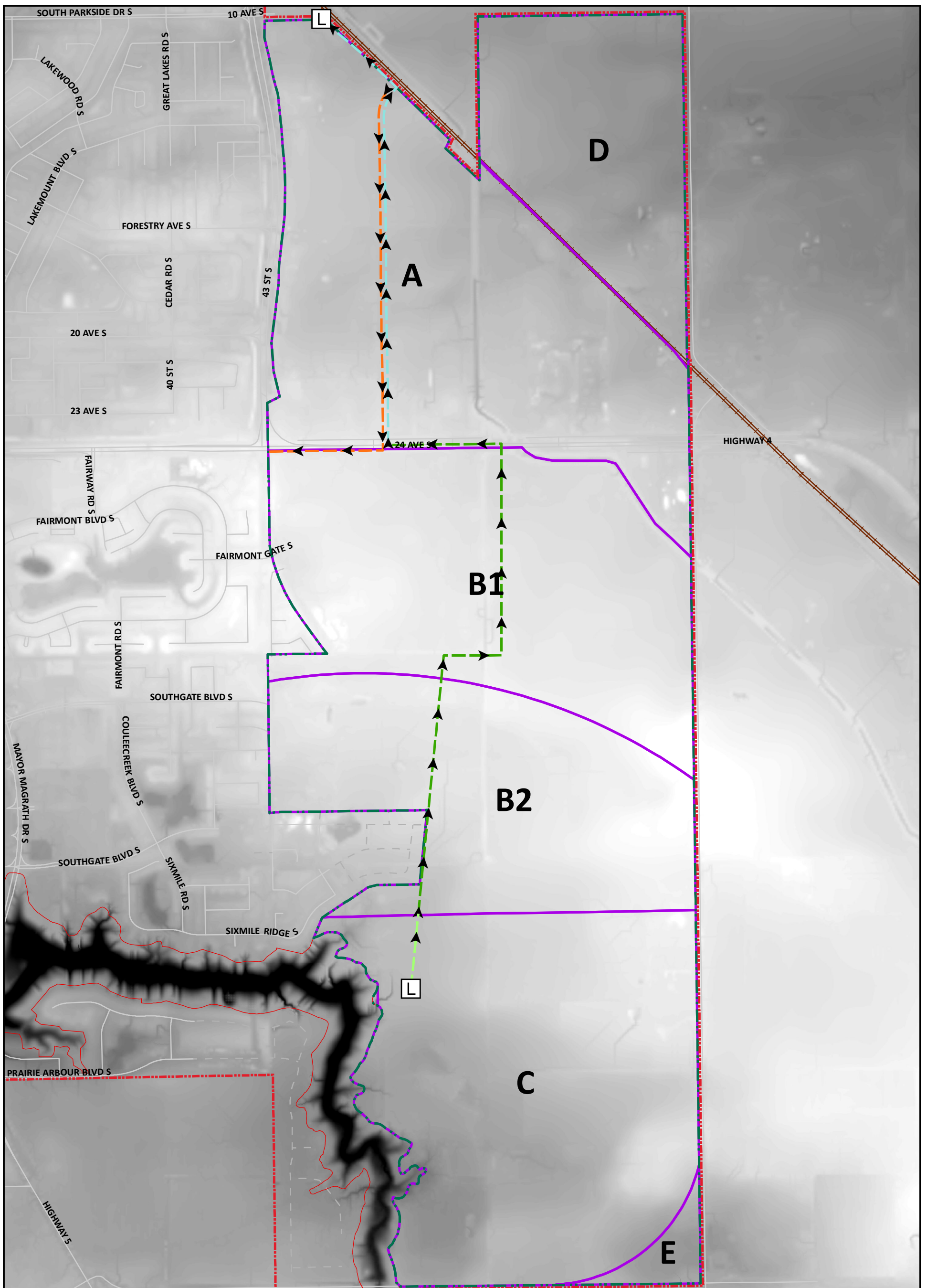
Further planning and evaluation to identify a preferred off-site sanitary sewer route will be completed as part of future Outline Plans for the SEASP area and as part of the City’s Capital Improvement Program.

#### 7.3.4 Objectives

- a) Provide a sanitary sewer system that serves the SEASP area.

#### 7.3.5 Policies

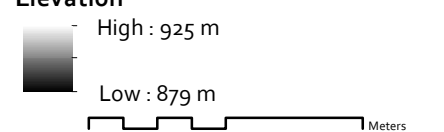
- a) The sanitary sewer system shall meet the standards and best practices that exist at the time of development.
- b) Subsequent Outline Plans shall utilize an EPASWMM model of the distribution system for the plan area using the wastewater generation rates shown in the Utility Servicing Plan.
- c) Within the SEASP area, Servicing Areas A and B1/B2 will be serviced by a gravity sanitary sewer trunk. Servicing Area C will require construction of a lift station prior to development commencing in this area. The exact location of this lift station shall be determined at the Outline Plan stage.



**Legend**

- City Boundary
- Southeast Area Structure Plan
- Servicing Area
- Safe Development Setback Line
- Railway
- Existing Roads
- Planned External Roads
- Force Main
- Gravity Main
- Option A - 10 Ave. Lift Station Route
- Option B - Scenic Dr. Route
- Lift Station

**Elevation**

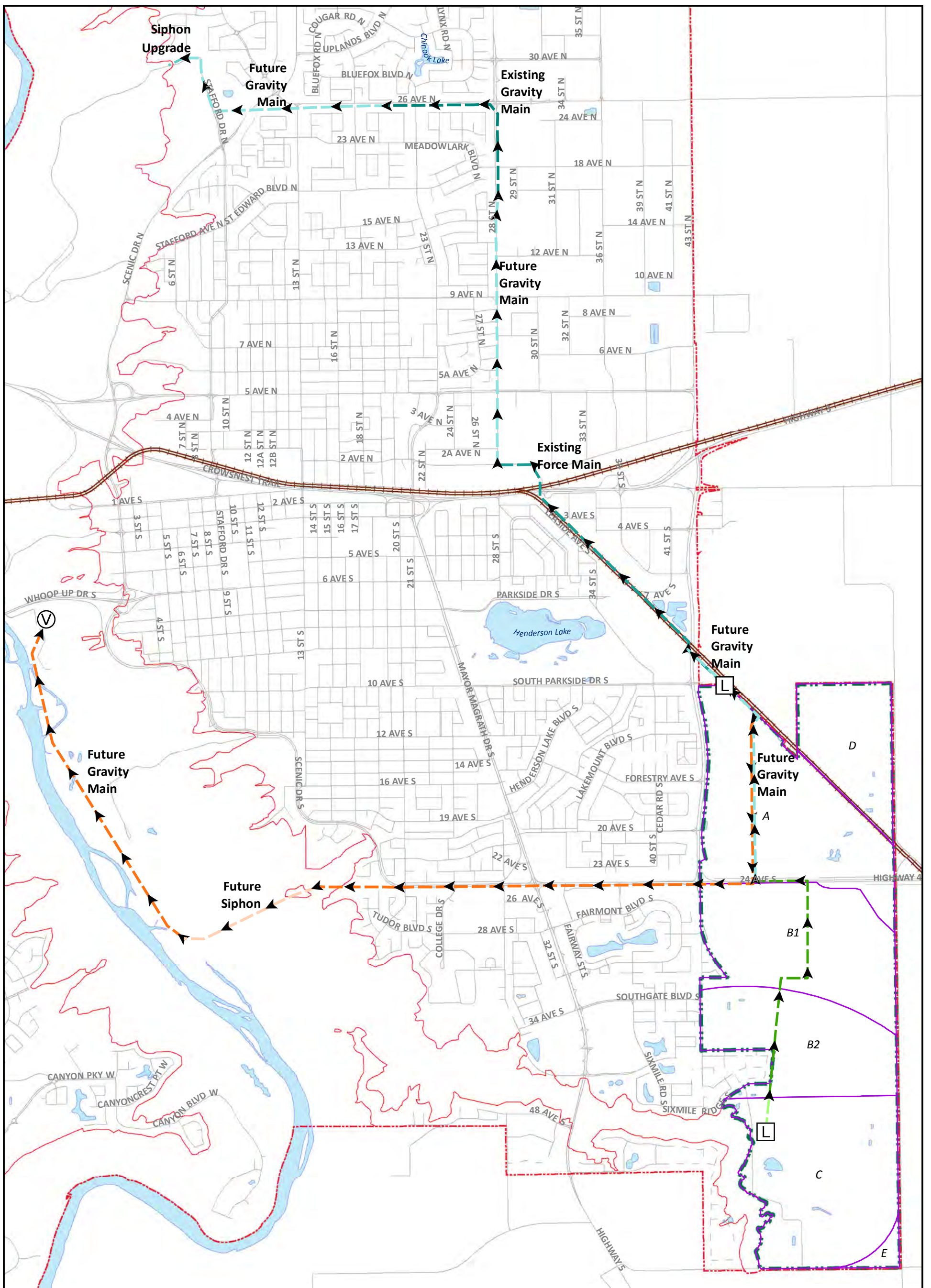


# MAP 12 - Sanitary Servicing

The Southeast area will be serviced by a standard sanitary sewer system following the alignment of collector and circulation roads, either draining to a regional lift station located in the area of 10 Ave. S and 43 St. S or to a system going west along Scenic Dr S.







**Legend**

- City Boundary
- Southeast Area Structure Plan
- Hydrology
- Planned External Roads
- Railway
- Existing Roads
- Safe Development Setback Line
- Proposed Future Force Main
- Proposed Future Gravity Main
- Option A: Future Gravity Main
- Option A: Siphon Upgrade
- Option A: Existing Force Main
- Option A: Existing Gravity Main
- Option B: Future Gravity Main
- Option B: Future Siphon
- Servicing Area
- Lift Station
- Siphon Vault

0 125 250 500 Meters

## MAP 13 - Sanitary Sewer Tie-In

Offsite sanitary servicing will consist of one of two potential sewer system route options. Option A requires construction of a lift station near 10 Ave. S and 43 St. S to pump wastewater north. Option B requires construction of a gravity fed sewer heading west along Scenic Drive S, into the river valley, and north to a sewer vault located south of Whoop Up Drive.



- d) Available off-site sanitary sewer capacity that currently exists in South Lethbridge shall be utilized for the initial phases of development in the SEASP.

## 7.4 Water Distribution System

The plan area will be serviced by a standard urban water distribution system following the alignment of the arterial, collector and circulation roadways. Water will be supplied from connections to the current and future water distribution system which will service the plan area (see Map 14).

### 7.4.1 Design Criteria

The City of Lethbridge water distribution system is divided into two pressure zones, with all of North and South Lethbridge contained in one pressure zone. One set of high-lift pumps at the Water Treatment Plant deliver water through transmission lines to the South Lethbridge Reservoirs for distribution across South Lethbridge. The plan area is primarily served by the existing Southeast Reservoir located in Southgate.

Calculation of water demands used a population-based standard for the primarily residential area south of 24 Avenue S. The demand for the area north of 24 Avenue S will be conservatively estimated based on the assumption that sewage generation rates are the same as water demand rates.

Using Provincial sewage flow generation guidelines for commercial and industrial land uses (0.45 L/s/ha) and 0.35 L/s/ha, respectively), Tables 2, 3 and 4 in the Utility Servicing Plan (Technical Documents Appendix) shows the corresponding demands for the identified “Servicing Areas”—keeping in mind that these areas do not necessarily correspond to future Outline Plan areas.

Sizing and alignment of the water distribution system will be refined during the Outline Plan process. It is recommended that the Outline Plan include an Environmental Protection Agency NETWORK Model (EPANET) of the distribution system for the plan area using the maximum day demands shown in the Utility Servicing Plan, and a fire flow of approximately 400 L/s (based on a typical large-format commercial building).

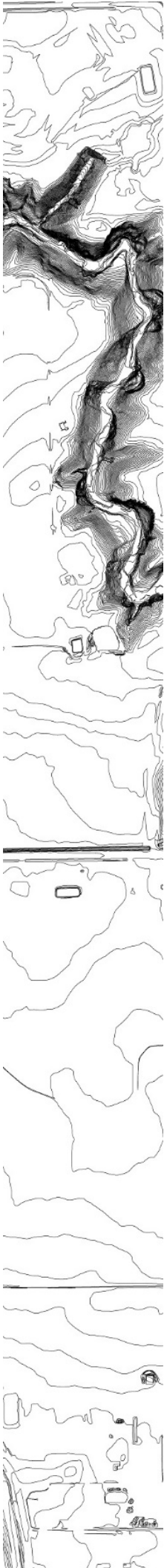
Fire flows required for commercial and industrial areas should be calculated using the Fire Underwriters Survey guidelines and reasonable projections for building sizes, construction materials and separations.

Detailed design at the Outline Plan stage should also incorporate system looping, such that at least two connections are provided to the existing distribution system for each phase of development. All water distribution infrastructure will follow the standards in place at the time of development.

### 7.4.2 Objectives

- a) Provide a water distribution system that is efficient and effective.



- 
- b) Provide a water distribution system that is resilient to interruptions.

#### 7.4.3 Policies

- a) Water to the plan area will be provided from the existing Southgate reservoir.
- b) The water distribution system shall meet the standards and best practices that exist at the time of development.
- c) Fire flows required for commercial and industrial areas shall be calculated using the Fire Underwriters Survey guidelines and reasonable projections for building sizes, construction materials and separations.
- d) Subsequent Outline Plans shall include an EPANET model of the distribution system for the plan area using the maximum day demands shown in the Utility Servicing Plan.
- e) All phases of development shall be looped, including at least two connection points to the existing distribution system.

## 7.5 Shallow Utilities

### 7.5.1 Natural Gas

There are existing gas distribution lines throughout the plan area providing service to homes, businesses and institutions. As development in the area progresses over time, these lines can readily be relocated or abandoned as needed.

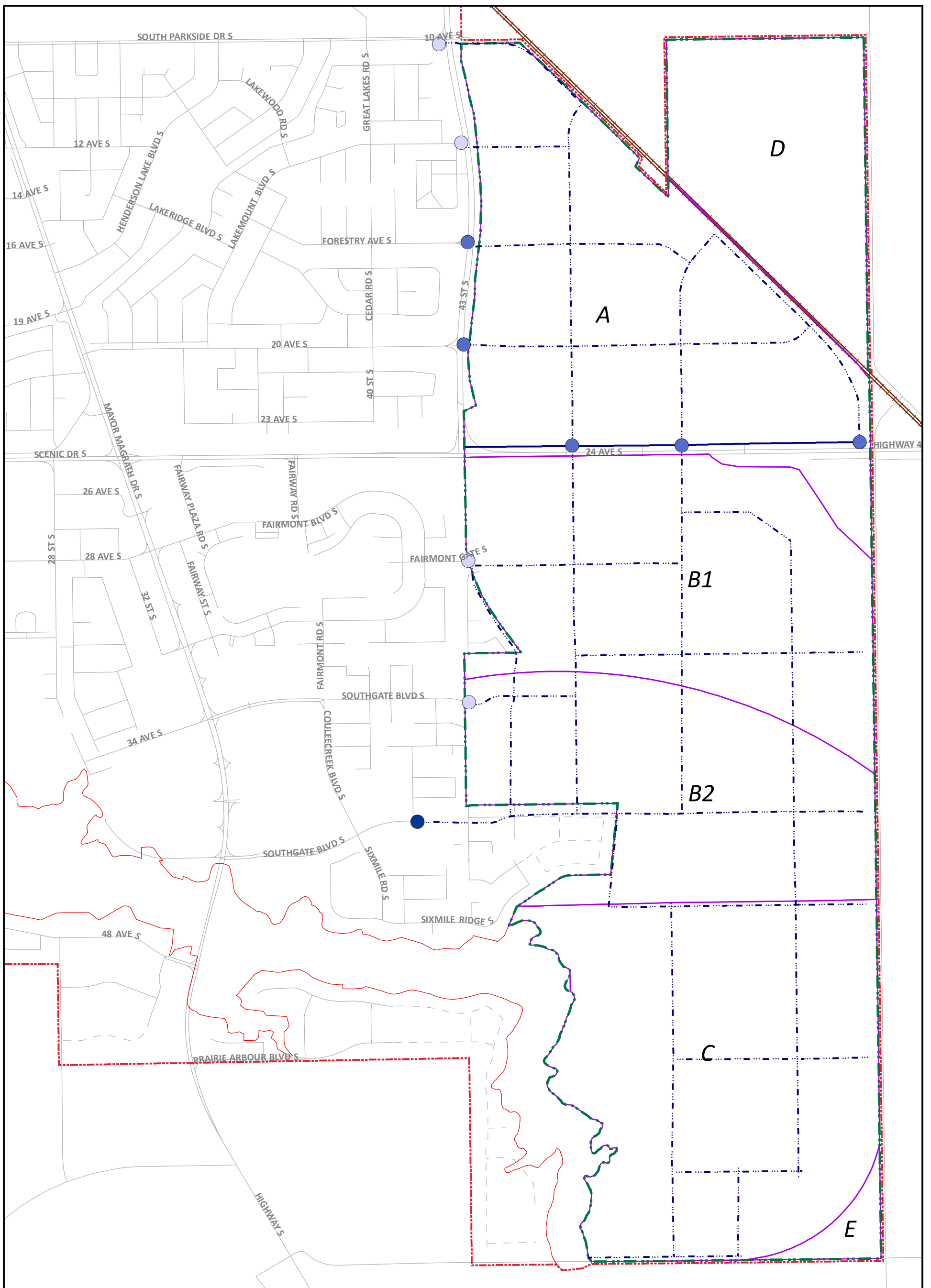
### 7.5.2 Electrical Utility

The City of Lethbridge will service the area through a network of feeder lines extending from the existing substation located at the intersection of Highway 4 (24 Avenue S) and 43 Street S. To do so, lines will follow the network of arterial and collector roadways discussed in Section 6 of the SEASP. Sufficient utility rights-of-way will be required at the time of subdivision and must be accounted for. To do this the development of an Electrical Distribution Concept Plan will be required to be completed by an electrical engineering consultant on behalf of the developer as part of subsequent Outline Plans. Pathways used to access electrical infrastructure will be designed to support the weight of service equipment.













### 7.5.3 Communications

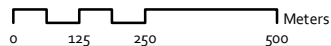
In our world, access to technology and innovation are important aspects of resident quality of life, business success, and the efficient delivery of municipal services. Ensuring that our residents' homes, places of work and public infrastructure can accommodate future technological changes and promote connectivity is therefore necessary. Through the ICSP/MDP the City of Lethbridge is committed to ensuring an enviable quality of life for our





**Legend**

-  City Boundary
-  Southeast Area Structure Plan
-  Servicing Area
-  Safe Development Setback Line
-  Railway
-  Existing Roads
-  Planned External Roads
-  Existing Water Distribution
-  Proposed Water Distribution
- Diameter of Existing Water Line**
-  200 mm
-  250 mm
-  350 mm



# MAP 14 - Water Servicing Concept

The Southeast area will be serviced by a standard urban water distribution system following the alignment of arterial, collector, and circulation roadways. Water will be supplied from connections to the current and future water distribution system at points along 43rd Street, 24th Avenue, Southgate Blvd, and Fairmont Gate.





residents, promoting an environment of entrepreneurship and innovation, and delivering high quality municipal services that meet the needs of today and generations to come.

Understanding the rapid pace at which technology changes, the growing expectations of residents and businesses to access the latest technology and the lasting impact that technological infrastructure has on the built landscape, it is important to plan for the future. However, it is difficult to anticipate future technological innovation. Consequently, there is a need to be open minded about the future and to create an environment that is open and adaptable.

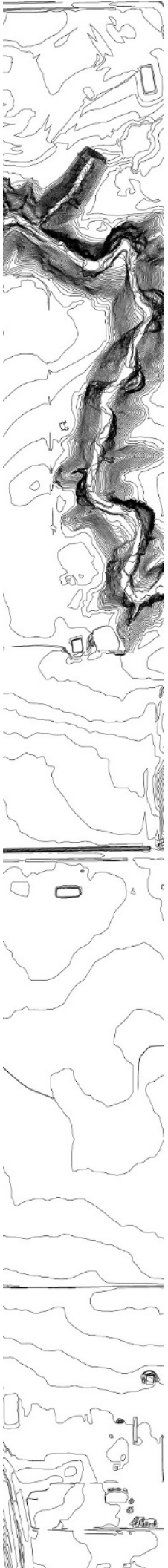
Subsequent Outline Plans are therefore best positioned to specifically address communications, infrastructure and service provision at that stage - keeping the broad goals of connectivity and innovation in mind. This can include addressing specific infrastructure requirements, such as: requiring light standards that can support the weight of attaching private utility infrastructure, parks and buildings that support public Wi-Fi, residential subdivisions that facilitate the “internet of things” or smart business parks with shared fibre optic cable.

Traditional telephone and cable servicing within the plan area will be facilitated by extensions to the existing system and will largely take place through trench installation. Other communications facilities, such as telecommunications towers, shall be planned for to the greatest extent possible at the Outline Plan stage in conjunction with telecommunication service providers. Large pieces of infrastructure, such as towers, are encouraged to be sited in locations that minimize visual impacts on residential areas. However, it should be noted that approval of such infrastructure is currently the responsibility of the federal government and the City has limited ability to influence their decision.

#### 7.5.4 Objectives

- a) Allow for the necessary shallow utilities to adequately service the plan area.
- b) Ensure infrastructure adheres to the level of service prescribed by the City of Lethbridge, provincial and federal standards.
- c) Provide for the establishment and expansion of telecommunications infrastructure that supports the principles of connectivity and innovation.
- d) Remain open to future technological innovation, promoting it where it supports improvements to residents’ quality of life, business success and the efficient delivery of municipal services.
- e) Locate shallow utilities in a manner that will be serve proposed development while respecting City design standards.
- f) Where possible, seek to minimize the visual impact of telecommunications infrastructure on residential areas.





#### 7.5.5 Policies

- a) Shallow utilities shall be designed to the level of service that is established by the City of Lethbridge and the provincial and federal governments.
- b) Further details of shallow utility servicing shall be provided at the Outline Plan stage. In consultation with City Departments and utility providers, this will include the development of an Electrical Distribution Concept Plan by a qualified electrical engineering consultant (on behalf of the developer) and the allocation of sufficient right-of-ways.
- c) Subsequent Outline Plans shall further investigate the principles of connectivity and innovation to ensure that advancements in technology that support resident quality of life, business success and efficient municipal service delivery are incorporated through appropriate infrastructure and utility provision.
- d) The placement of major, visible telecommunications infrastructure (i.e. large towers, switching hubs) in commercial and business industrial areas and on public lots, as opposed to in residential areas shall be encouraged.
- e) Dialogue with appropriate government agencies and communication providers to adequately plan for new telecommunication infrastructure as best and as far into the future as possible, shall be encouraged at subsequent planning stages.

# 8.0 Implementation

## 8.1 Outline Plans and Sequence of Development

Additional comprehensive planning is required beyond the policies outlined in this ASP before subdivision and development may proceed within the plan area. In accordance with the practices of the City of Lethbridge, Outline Plans will be prepared and approved by the Municipal Planning Commission (MPC) for sub-areas identified in this Plan (see Map 15).

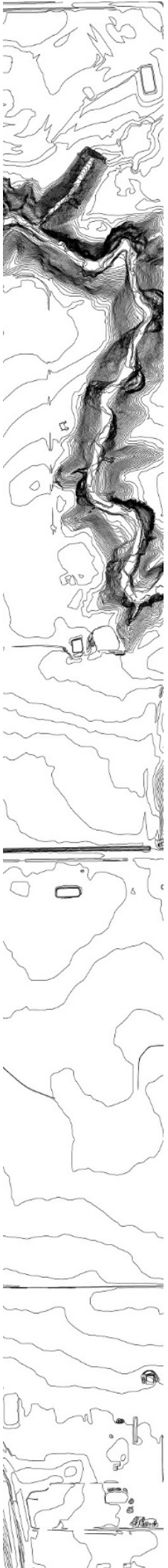
The Planning Principles identified in Section 1.3 of this ASP, call for “financially sustainable infrastructure delivery”. In order to fulfill this principle, viable servicing strategies for sanitary sewer, storm sewer, water distribution and major roads, along with affordable financing mechanisms, must be identified in advance of proceeding with outline plans. The servicing and financing strategies need to consider both the ultimate servicing of the entire area, as well as interim servicing and financing of development stages as they progress. This is directly addressed in the current ICSP/MDP, which states under section 6.4.4, policy 9 that “development in new residential growth areas, not already addressed in existing policies, is to occur as financing and infrastructure is available.” In addition, the initiation of an outline plan will be subject to growth principles that support logical, orderly and financially sustainable community growth.

Within the SEASP area the Outline Plan areas will be developed in a logical, sequential order that ensures financial responsibility and an adequate supply of serviced land of appropriate uses. The boundaries of Outline Plans and the order in which they are to be prepared are shown on Map 15. In this ASP the first letter of an Outline Plan area describes its primary land use and the first number describes the sequential order it is to be developed in. Outline Plan areas of different primary land uses can be developed in an independent sequence from one another. For example, it is possible that Outline Plan areas R1a and E1 can developed concurrently with one another, but development in Outline Plan R2 must wait until R1a is almost complete. In the case of Outline Plan areas R1a and R1b, the second lower case letter denotes that both areas will likely be developed within the same timeframe, but that it is possible that both areas may proceed under two different Outline Plans.

In previous ASPs, Outline Plan areas and their sequence of development were, for the most part, based upon local market conditions and land ownership. This method did not consider other issues with the same, or perhaps greater, importance such as overland drainage and catchment basins or the availability of services to the site. This sometimes resulted in inconsistent development that could have been serviced more efficiently. Therefore, the sequence of development and the size of each Outline Plan area within the SEASP area has been determined based upon a number of criteria, including:

- Development constraints (man-made and natural constraints such as topography, pipelines, transmission lines and irrigation infrastructure);

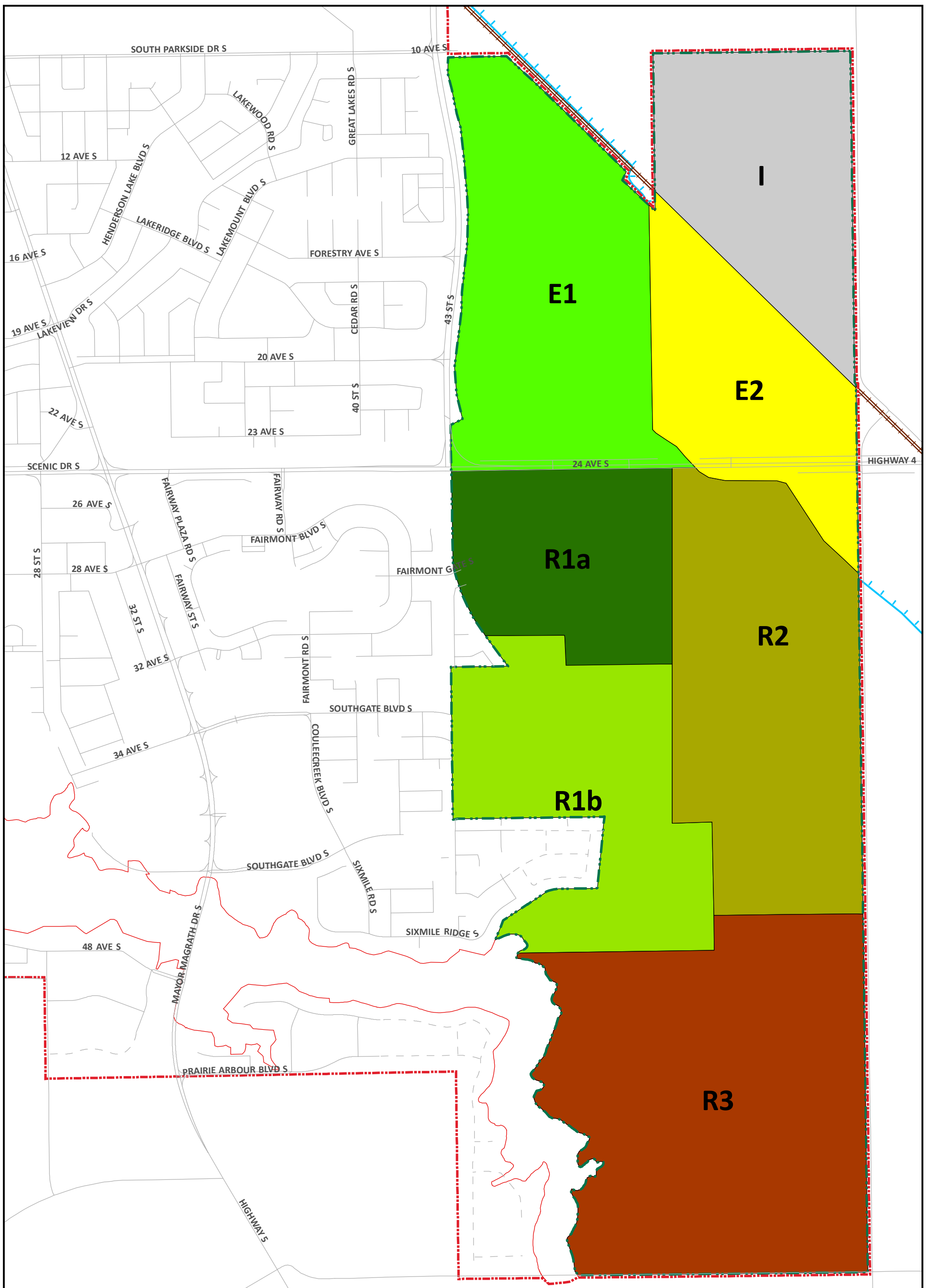










- Market demand for anticipated future land use;
- Timing and sequence of servicing (stormwater, sanitary sewer, water) upgrades;
- Timing and sequence of transportation upgrades; and,
- Development pressure.

A technical analysis on the part of the various City departments, including Planning and Development Services, Urban Construction, Transportation, and Electric reviewed the timing of infrastructure and servicing to arrive at different parts of the plan area as well as market demand conditions. This information was then overlaid on the catchment areas, landownership information and the proposed arterial and collector road network. The location, general size and sequence of the Outline Plans were then determined.


Based on the technical analysis, development in the SEASP will generally commence in areas adjacent to existing neighbourhoods of Fairmont and Southgate to the west and in proximity to the alignment of 24 Avenue S. This area has best access to existing servicing capacity and is a logical extension of urban development. From this point, development will move south, north and west. As noted in previous sections in this document, the area located north of the railway, shown as “Semi-Rural” on Map 7 and as Outline Plan Area I on Map 15 will not receive additional utility servicing. Development in Outline Plan Area I will still require the completion of an Outline Plan, however, this area can be developed out of sequence and independent of the other Outline Plan Areas, as it can be developed in a limited function that does not require improvements to access or the extension of utility servicing. A summary of the servicing requirements for each Outline Plan area and how these requirements affect the sequence the Outline Plans are developed are summarized in Table 7.

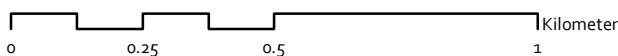


**Legend**

-  City Boundary
-  Southeast Area Structure Plan
-  Railway
-  Safe Setback Line
-  Existing Road
-  Planned External Roads

**Phase**

-  E1
-  E2
-  I
-  R1a
-  R1b
-  R2
-  R3



**MAP 15 -  
Outline Plan  
Sequence**

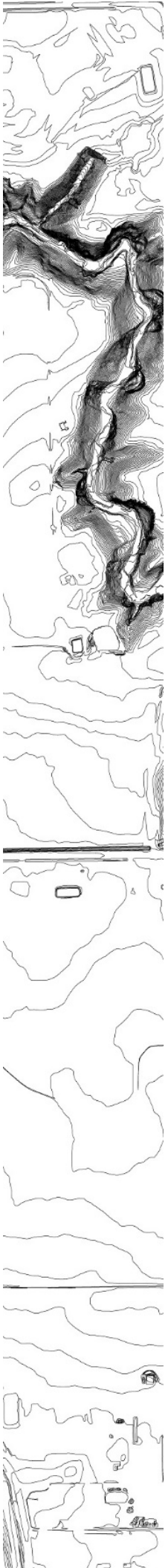






Table 7 Outline Plan Area Servicing Sequence & Requirements Matrix

Outline Plan Area Servicing Sequence & Requirements Matrix					
Outline Plan Area	Location	Water Servicing	Sanitary Servicing	Storm Servicing	Arterial Roads
I	NE of the railway	Not serviced	Not serviced	Not Serviced	Connects to 58 St S via existing at grade rail crossing
E1	N of 24 Ave S and W of Canal	Connects to existing water mains at 43 St S and across to Lakeview neighbourhood	<b>Option 1</b> Requires the regional liftstation located near 10 Ave S and 43 St S and offsite servicing connecting to the North Siphon <b>Option 2</b> Requires the construction of the Scenic Dr gravity main to 43 ST and siphon through the river valley	Requires storm water pump station connected to storm trunk extended to 24 Ave S across areas R1a and R1b from the Six-Mile Coulee Outfall	Access from either existing 43 St S or 24 Ave S
E2	N and E of canal and S of railway	Requires water mains extended from area E1	Requires Sanitary Sewer Extended through area E1	Serviced by storm water pump station in area E1	Arterial Access to 24 Ave S. May also access through area E1
R1a	S of 24 Ave S and E Fairmont	Connects to existing water mains in 43 St S and into Fairmont	<b>Option 1</b> Requires sanitary trunks extended from the regional liftstation through area E1 to 24 Ave <b>Option 2</b> Requires the construction of the Scenic Dr. gravity main to 43 ST and siphon through the river valley	200 l/s of storm capacity is available through the Southgate system. This capacity will allow a total of 65ha of development shared between area R1a and R1b before the Six-Mile Coulee Outfall is required. Area R1a will require storm sewer trunks to be extended across area R1b	Arterial Access to 24 Ave S
R1b	E of Southgate	Connects to existing watermains in Southgate	Approximately 25 ha of sanitary sewer capacity is available through the existing Southgate system. After existing capacity is used sanitary trunks required to be extended across area R1a	200 l/s of storm capacity is available through the Southgate system. This capacity will allow a total of 65ha of development shared between area R1a and R1b before the Six-Mile Coulee Outfall is required	Arterial access through Southgate to Mayor Magrath Drive S and through area R1a to 24 Ave
R2	E edge of City and S of Canal	Connect to watermains extended through R1a and R1b	Connects to Sanitary trunk running along the east edge of R1a and R1b	Six-Mile Coulee Outfall is required. Connects to Storm trunk running along the east edge of R1a and R1b	Arterial access to 24 Ave through collector roadway running along the east edge of R1a and R1b
R3	Southerly limits of the City	Connects to watermains extended through R1b and R2	A liftstation is required to be located in the Northwest corner of area R3 The liftstation will pump into sanitary trunk through R1a and R1b	Connects to Six Mile Coulee Outfall near the North West corner of area R3	Initial road access through R1b and R2



It may be possible for development to proceed non-sequentially (i.e. not following the sequence outlined in this Plan), at the discretion of the City of Lethbridge and with the developer assuming the risk and financing the cost of extending services to the area in question. This option of non-sequential development would require a significant amount of negotiation between the developer and the City. The City of Lethbridge reserves the right to review and revise the sequencing of Outline Plans from time to time and as needed.

Full development of the SEASP area will take a number of decades. As such, development may occur unevenly as the area transitions from the existing uses to its future uses. To ensure that existing uses will not be unduly impacted by adjacent development as the area is transitioning, Outline Plans are required to address how the transition from existing to future uses will occur. An appropriate transitional area that is sensitive to the existing agricultural lands beyond the plan area will also be conceptualized in future Outline Plans.

In order to ensure that the existing road network can manage and integrate with new development, each Outline Plan will evaluate the impact that urban development in the Outline Plan area will have on the road network prior to adoption. It is preferred that this evaluation be completed in the initial stages of the Outline Plan process to reduce any unnecessary expenditure.

While the SEASP has been prepared by the City of Lethbridge, the costs associated with its preparation will be recovered by the City in subsequent planning stages from the associated landowners and / or developers. Completion of the Outline Plans will also be the responsibility of these parties, as they are better positioned to respond to the local market conditions. Review and approval of these Outline Plans will still be at the discretion of the City of Lethbridge and the Municipal Planning Commission (MPC). Outline Plans will also provide more specifics with regard to land use, individual development phases with the Outline Plan, the local roadway network and the provision of municipal infrastructure as described in the City of Lethbridge Design Standards.

Given the fragmented landownership in the plan area, it is important to ensure effective communication and transparency between parties initiating Outline Plans and other landowners within the given Outline Plan area boundary. All landowners within an Outline Plan area shall be given equal opportunity to participate in an Outline Plan, and the City of Lethbridge shall ensure that when a developer(s) initiates an Outline Plan, that a process for information sharing is developed, adhered to and documented in the Outline Plan document.

It is also important to stress that Outline Plans must adequately address the interface between Outline Plan areas, existing development and the adjacent land uses in Lethbridge County. Outline Plans will be required to address how they intend to transition amongst planning areas, including the transition of infrastructure, open and park space, land uses and public realm design.

#### 8.1.1 Objectives

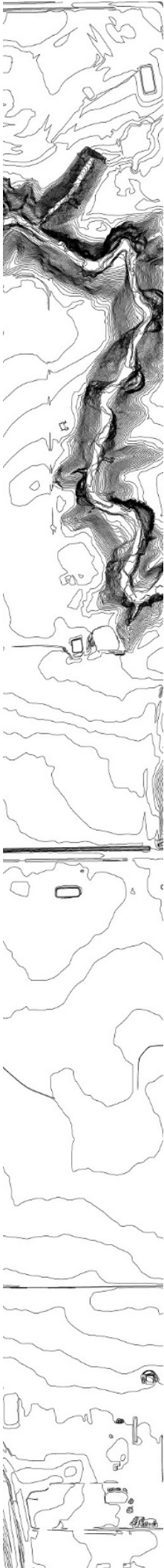
- a) Provide more detailed and comprehensive planning for the SEASP through Outline Plans.

- b) Create Outline Plans that are subject to growth principles that support logical, orderly and financially sustainable community growth.
- c) Ensure that new growth areas are developed in an efficient and cost-effective manner, minimizing impacts on adjacent land uses.

#### 8.1.2 Policies

- a) The cost the costs associated with the preparation of this ASP will be recovered by the City in subsequent planning stages from the associated landowners and / or developers. The method and stage (i.e. Outline Plan stage, Subdivision stage) at which this will be recovered will be determined through subsequent Outline Plans.
- b) Work on drafting Outline Plans cannot be initiated until staging and financial strategies for delivering the major infrastructure required for a given Outline Plan area have been identified. The strategies must be sufficiently detailed to fully describe infrastructure requirements and the associated costs at each stage in order to provide a high probability of success for the plan.
- c) Outline Plans shall be undertaken sequentially as shown on Map 15. Outline Plans shall be undertaken by interested developers or landowners to provide more comprehensive planning, prior to the subdivision and development of land.
- d) Outline Plans may proceed non-sequentially at the discretion of the City of Lethbridge. If approved, the developer shall assume the cost and risk of extending services to the Outline Plan area in question. This includes capital costs in addition to necessary maintenance costs.
- e) The further planning and semi-rural development of Outline Plan Area I (as shown on Map 15) will require the completion of an Outline Plan. However, development of this area can proceed non-sequentially and independent of the other Outline Plan areas shown in this document.
- f) The City of Lethbridge shall reserve the right to review and revise the sequencing of Outline Plans from time to time and as needed.
- g) Outline Plans will address how the transition from existing uses to future uses occurs.
- h) At the commencement of each Outline Plan, a traffic impact assessment will be conducted. This assessment will study the impact that future development in the Outline Plan area will have on the existing road network and other requirements as specified in the City of Lethbridge Traffic Impact Assessment Standards.



- 
- i) Development of uses that provide necessary municipal or utility services either to the SEASP area or the City (e.g., electrical substations) may proceed within the SEASP area prior to the adoption of an Outline Plan for its specified location.
  - j) Outline Plans shall be completed according to City of Lethbridge practices or as otherwise agreed to by the City of Lethbridge.
  - k) Development in Outline Plan Area R3 cannot commence until the sanitary lift station in the southern portion of the plan area is constructed and operational, as discussed under Section 7.3 of this ASP.
  - l) Prior to the commencement of each Outline Plan, the proponent of the Outline Plan shall provide written documentation indicating which of the landowners within the Outline Plan area boundary endorse the proponent to prepare an Outline Plan.
  - m) Prior to the commencement of each Outline Plan, the proponent of the Outline Plan shall demonstrate an information sharing process that is to the satisfaction of the City of Lethbridge. This information sharing process will ensure that all landowners within the Outline Plan area boundary are regularly and adequately notified of the planning process from initiation through adoption of the Outline Plan.

## 8.2 New or Modified Land Use Districts and Overlays

The implementation of additional land use districts or land use overlays through the Land Use Bylaw would greatly support the planning goals and policies that are laid out in this ASP. Existing land use districts could also be modified in order to accommodate the same function, but where relatively minor changes are needed to do so. Such measures would provide a regulatory structure that would either encourage or discourage certain types of development from becoming established in the plan area. This will entail:

### Land Use Overlays to Establish Attractive Commercial Gateways into the City.

Commercial and industrial land use districts are used on many of the City's gateway corridors that are located at entrances into the city, such as Mayor Magrath Drive S and the Crowsnest Trail W. While these districts regulate the function and land use of these corridors, they do little to mandate an attractive and inviting design at the City's entrances. A new land use overlay will maintain the required commercial and industrial land use districts, but will expect a higher degree of attention paid towards landscaping, the built form of buildings and how the public and private lands interact with one another.

Within the SEASP area, this district will be used for the development of the 24 Avenue S Gateway Corridor in order to accomplish the goals of this commercial corridor as laid out in Sections 4 and 6 of this ASP document.

**Land Use Districts Designed Specifically for Knowledge-based Employment.** Under the current Land Use Bylaw, such uses are applicable under certain industrial or commercial

land use districts. However, under these existing land use districts they are also combined with other industrial or commercial uses, such as manufacturing or retail stores. These other uses are not generally conducive towards attracting development for knowledge-based employment, which often require aesthetically pleasing areas that are separated from other uses that may produce offsite externalities.

Currently in Lethbridge, there are no land use districts that are geared exclusively towards the development of knowledge-based employment, however, examples of such land use districts are commonly found in other municipalities that have designated areas for similar research/technology/office parks.

Within the SEASP area a new land use district that is designed specifically for knowledge-based employment would be used for the development of nodes of such employment in the industrial or commercial areas of this ASP's land use concept. Such a land use district could also be used in other areas of the City, such as the West Lethbridge Employment Centre and is also discussed in the West Lethbridge Employment Centre ASP, approved in 2013.

#### 8.2.1 Objectives

- a) Provide new land use overlays or land use districts that assist in the development of the SEASP area as intended in this ASP through its goals and policies.
- b) Modify existing land use districts to assist in the development of the SEASP area as intended in this ASP through its goals and policies.

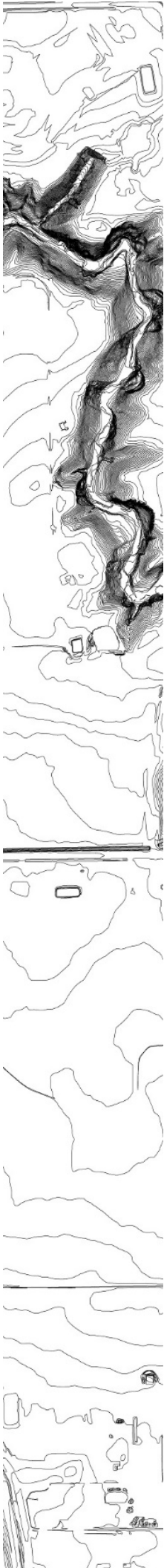
#### 8.2.2 Policies

- a) A new land use overlay shall be developed by City staff with the intention of establishing attractive gateway corridors into the city. This new land use overlay shall reflect the objectives and policies that were identified in sections 4 and 6 of this ASP. Within the context of the SEASP this new land use overlay is intended for use in what has been identified as the "24 Avenue S Gateway Corridor". This new overlay will be drafted as part of the OP process by the developer and will be incorporated into the Land Use Bylaw prior to the adoption of the OPs for OP areas R1A, R2, E1 and E2.
- b) The creation of a new land use district that is more conducive towards the establishment of knowledge-based employment, offices or business parks than current land use districts shall be considered at the Outline Plan stage if, based on the proposed land use, there is a need for this type of development in the community. Such a land use district would need to be brought forward to City Council for their approval

### 8.3 Existing Land Uses

At the time the SEASP was prepared, the plan area contained a number of commercial, business industrial, institutional, recreational residential and agricultural land uses. These land uses are allowed to continue until the parcel they are currently located on is developed. They may also expand providing they are in compliance with municipal and other necessary





legislation. Any allowable expansion must not introduce additional land uses beyond what the zoning allows.

As utility servicing is developed within the plan area, property owners will be given the opportunity to connect their properties as these utility services approach, in compliance with the ultimate development of utility services shown in the Outline Plan. Note that fees will apply where properties are connected to utility servicing, as the connection is at the expense of the property owner. Property owners will be notified of such costs by the appropriate department in advance of service delivery.

#### 8.3.1 Objectives

- a) Provide clarity to property owners regarding their rights as landowners within the plan area and their ability to continue and expand upon existing development.
- b) Ensure that existing land use is compatible with the orderly development of the plan area in accordance with the SEASP and ICSP/MDP.

#### 8.3.2 Policies

- a) An existing land use shall be defined as a land use that, at the time the SEASP is adopted by Council, has been developed in the boundaries of the SEASP area and is considered to be in compliance with all municipal regulations.
- b) Existing land uses and structures within the plan area, that have been established prior to the adoption of the SEASP, shall be permitted to continue to operate as they have in the past.
- c) The expansion of existing land uses may occur as governed by the LUB, provided that new land uses are not introduced.
- d) If a proposed expansion of an existing land use is neither a permitted nor a discretionary use under the existing land use district for a given parcel (i.e. the land use was developed prior to annexation and existing land use has been “grandfathered” in), an application to amend the LUB to a Direct Control (DC) zoning shall be required to address the specific limitations on the development to ensure that it cannot be modified from the original intent, until such time as the property is ready to undergo full urban development.
- e) To limit any additional parcel fragmentation in the SEASP area, any Direct Control (DC) district that is approved to facilitate the expansion of an existing land use (see Section 8.3.2 Policy (d) shall restrict additional subdivision from occurring on the associated parcel.
- f) Standard procedures for Land Use Bylaw amendments or development permit applications shall still apply for any existing development proposals. The approving authority shall retain the ability to approve, amend or refuse any such application.

## 8.4 Interim Land Uses

There is an interest amongst a number of land owners to proceed with development of their property in the near future. However, as with other undeveloped areas within the City, permanent development is not possible until further planning is complete and municipal services are available. These include both “hard services” such as adequate roadways, water mains, sanitary sewers, storm sewers and “soft services” such as fire, police protection and education.

Land will be sequentially made available for permanent development following the order indicated on Map 15, and according to the policies outlined in Section 8.1. Due to the available real estate market and the distance that some properties lay from servicing, the full urban development of some areas will be significantly far off into the future.

Previously, interim land uses that have been considered to be appropriate have been given approval but have not followed any kind of standard in their implementation. The policies in the SEASP governing interim land uses bring a standard for all such future interim uses to ensure that approvals for interim uses are applied as fairly and clearly as possible.

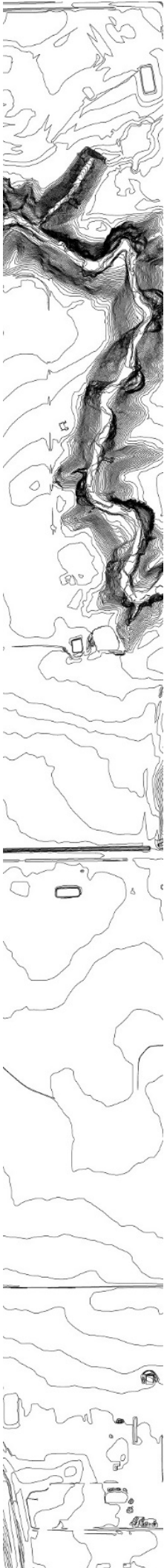
In order to provide area land owners with a provisional opportunity to develop their property in some form, but recognizing that this potential development must not interfere with the future urban development of the area and must not place a strain upon existing services, the SEASP provides policies to guide interim land uses, that are appropriate for the area, until such time as land is ready for broader development. These policies have been created to address the following issues:

**Land Use.** Other than existing development, interim development must avoid permanent structures and require minimal or no municipal additional servicing. Any development must also be relatively easy to remove once the land is ready for urban development and must incorporate minimal improvements to the site. Examples are mini-storage provided through shipping containers or recreational vehicle storage. Interim developments are still required to receive approval as per the Land Use Bylaw.

**Maximum Area of Property Devoted to Interim Development.** Interim uses cannot compose a substantial portion of a given property, as they begin to mirror permanent development and put a strain on unimproved services if they are allowed to become too substantial. The existing land use must be retained as the primary use on the site, until such time as the property is ready for urban development.

**Limiting Parcel Fragmentation.** Parcel fragmentation can pose challenges for future urban development to proceed in an efficient and consistent manner, as developers must acquire ownership of fragmented parcels or land owner consent. To limit future fragmentation and to facilitate the future development of the area, additional subdivisions within the plan area will be restricted.



A vertical strip on the left side of the page shows a topographic map of a river valley. The map features contour lines indicating elevation, a winding river channel, and some rectangular structures that could be buildings or infrastructure. The map is oriented vertically, matching the text layout.

**Maximum Amount of Interim Development Permitted in the SEASP.** There is the potential for existing infrastructure to be taxed beyond capacity if certain levels of interim development are exceeded. A situation such as this would also make it more difficult for urban development to eventually become cohesively established in the SEASP area. The City of Lethbridge will evaluate each proposed interim development application to ensure that existing infrastructure can support this interim development.

The SEASP policies that deal with these issues are intended to inform landowners and allow them a degree of flexibility in the operation and interim development of their properties. However, proposed interim developments that conform to the policies of this section are not guaranteed to be approved, nor supported by City Administration.

#### 8.4.1 Objectives

- a) Provide clarity to landowners regarding the potential for interim development of their land.
- b) Provide the opportunity for interim development that is feasible and beneficial for landowners prior to the full urban development of the plan area.
- c) Ensure that interim development is not premature and that municipal servicing capacity exists for such interim development to occur.
- d) Ensure that interim development will not become an obstacle to the future urban development of the plan area.

#### 8.4.2 Policies

- a) An interim use shall be defined as a land use that is not intended to be permanent and requires minimal improvements to the site. It must be relatively easy to remove once the land is ready for full urban development and shall not include permanent structures. An interim development shall not require additional or upgraded municipal servicing.
- b) If a proposed interim development is neither a permitted nor a discretionary use under the existing land use district for a given parcel, an application to amend the LUB shall be required. Such an application must be approved by Lethbridge City Council prior to the development of the intended use.
- c) Any Land Use Bylaw amendment proposed to facilitate an interim development must redistrict the land to a Direct Control (DC) district. This district will place specific limitations on the interim development to ensure that this development cannot be modified from the original intent until such time as the property is ready to undergo full urban development. Conventional zoning districts do not allow this type of control.



- d) Any Direct Control (DC) district that is approved to facilitate an interim development must restrict additional subdivision from occurring on the associated parcel. The purpose of this is to limit additional parcel fragmentation in the SEASP area.
- e) Standard procedures for LUB amendments or development permit applications shall still apply for any interim development proposals. The approving authority shall retain the ability to approve, amend or refuse any such application.
- f) The maximum amount of the total parcel that an interim use can utilize shall be 20%. For example, a 10 ha parcel would allow a maximum of 2 ha of the parcel to be used for interim uses, while the remaining 8 ha would be retained for the existing purposes.
- g) The maximum amount of land that can be used for interim land uses within the entire plan area is at the discretion of the development authority. It shall be based upon such factors as existing roadway capacity, existing servicing capacity and emergency services response.
- h) The City shall maintain the ability to impose any other restrictions through the Direct Control (DC) land use district or through any development permits that are issued for interim land uses.

## 8.5 Relationship to Existing South Gate Planning

Policies exist in the SEASP to provide for an effective transition between existing neighbourhoods and the future development phases of the SEASP area, including future road network and utility servicing. To ensure the coordination of planning in Southeast Lethbridge and clarity to area residents, the vision and policies of the SEASP shall supersede portions of the approved South Gate Area Structure Plan and South Gate Outline Plan as indicated in Map 16.

Following the adoption of the SEASP, both the South Gate Area Structure Plan and South Gate Outline Plan shall be amended to reflect the new hierarchy of planning documents. When this occurs, the South Gate Area Structure Plan and South Gate Outline Plan will apply only to phases of South Gate that have already been developed.

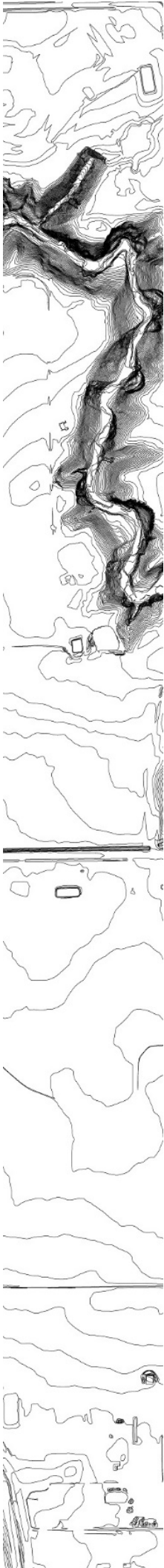
### 8.5.1 Objectives

- a) Effectively coordinate planning between existing development in South Gate and future development in the SEASP area.

### 8.5.2 Policies

- a) Both the South Gate Area Structure Plan and South Gate Outline Plan shall be amended to only apply to developed phases in South Gate, as shown in Map 16 of this document.



- 
- b) Undeveloped portions of the South Gate Area Structure Plan and South Gate Outline Plan as of the adoption date of the SEASP shall be subject to policies of the SEASP.

## 8.6 Performance Monitoring

Implementation of the SEASP is an ongoing, long-term activity and, as such, is prone to changes in the housing and job market, technology and service delivery. In order to ensure that the implementation of the Plan is proceeding in a feasible and sustainable manner, and that any potential problems are adequately addressed, the performance of this plan must be monitored at the completion of the development of each Outline Plan area. This monitoring should take place in the form of a report to City Administration, MPC and/or City Council and should discuss the following in relation to the SEASP and its goals:

### Previous Performance

- Identification of any Outline Plans that are in development or have been approved over the previous monitoring term.
- Any publically-funded projects that have occurred over the previous monitoring term.
- Any amendments to the SEASP that have occurred over the previous monitoring term.
- Any strengths or weaknesses of the SEASP that have been identified through the implementation process.
- Evaluation of how well the needs of the community are being fulfilled through the implementation of the SEASP.

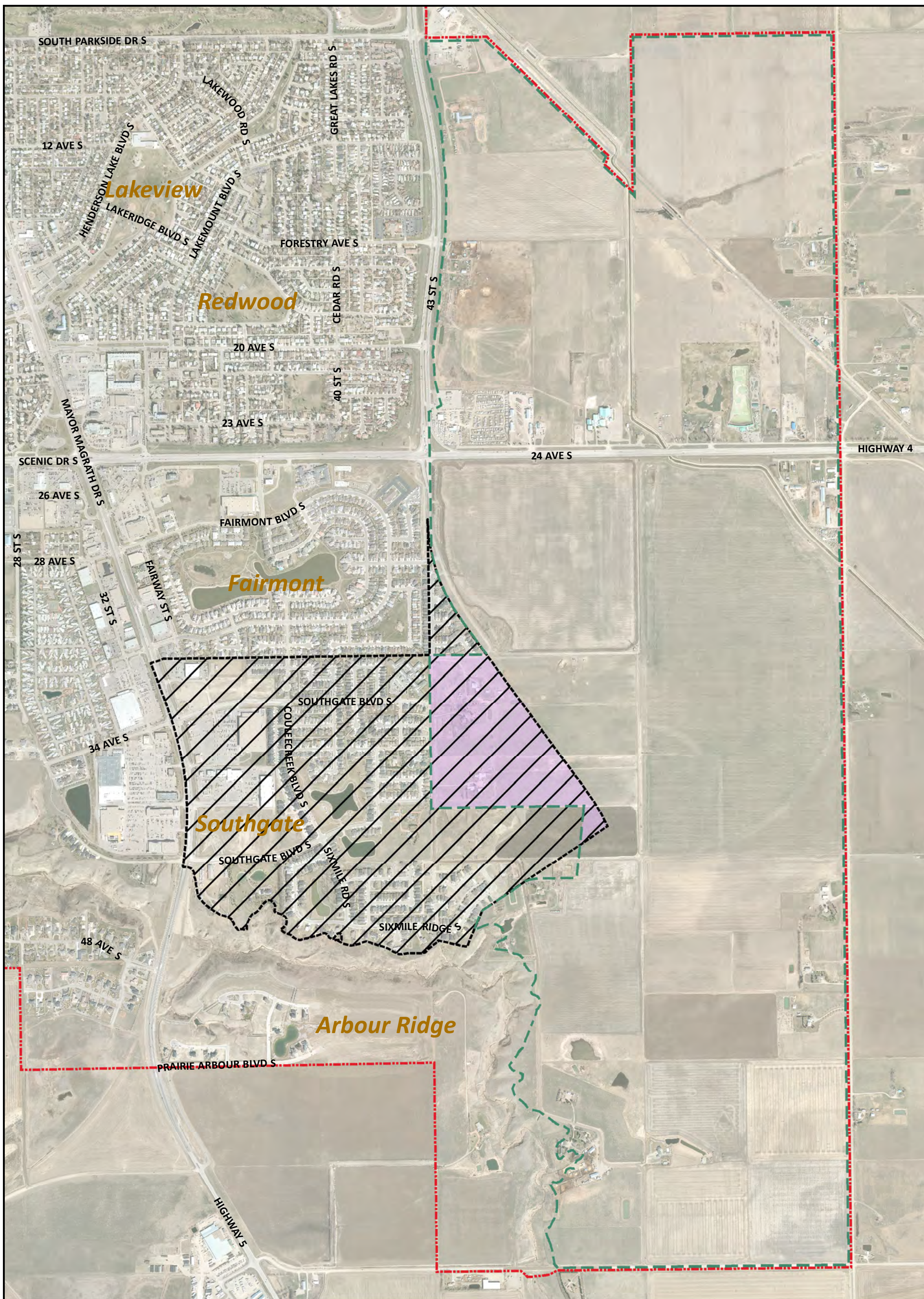
### Future Actions

- Development that is expected to occur over the next monitoring term.
- Any publically-funded projects which are likely to occur over the next monitoring term.
- Any recommended amendments to the SEASP.





In addition, the SEASP should be reviewed by Infrastructure Services and Community Services with each update to the Capital Improvement Program (CIP) in order to make certain that the infrastructure needs of the SEASP area are met to facilitate future growth.

### 8.6.1 Objectives

- a) Provide a method for continual monitoring of the SEASP implementation to ensure relevancy and that any problems that may arise are adequately addressed.
- b) Ensure that the infrastructure and service delivery needs to facilitate future growth in the plan area are understood and provided for.

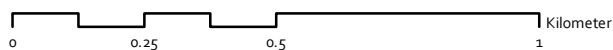


**Legend**

-  Southeast Area Structure Plan
-  City Boundary
-  South Gate Area Structure Plan
-  Superseded Area



# MAP 16 - Superseded Portions of Southgate ASP





#### 8.6.2 Policies

- a) A Performance Monitoring Report for review by City Administration, MPC and/or City Council shall be developed at the completion of development in each Outline Plan area.
- b) The Performance Monitoring Report shall discuss the performance of the SEASP and its implementation over the previous monitoring term. This shall include discussion on ongoing Outline Plans and development in the plan area, previous publically-funded projects and any SEASP amendments that have occurred. This shall also identify any strengths and weaknesses that have been identified through the implementation process and shall evaluate the SEASP in terms of how well it is meeting the needs of the community.
- c) The Performance Monitoring Report shall identify future actions that should occur in the SEASP implementation process. This includes discussion on future development that is anticipated to occur during the next monitoring term, any publically or off-site levy funded projects that are anticipated to occur over the next monitoring term and any recommended amendments to the SEASP.
- d) The SEASP shall be reviewed by Infrastructure Services and Community Services with each update to the Capital Improvement Program to identify any future infrastructure upgrades that are required in the plan area to facilitate future growth





# Appendices

Appendix A – Land Use Stats





Projected Land Use Statistics  
Southeast Area Structure Plan

	Outline Plan Area E1	Outline Plan Area E2	Outline Plan Area R1a & R1b	Outline Plan Area R2	Outline Plan Area R3	Outline Plan Area I	Total	% of GDA
<b>GROSS AREA</b>	<b>110.0</b>	<b>66.0</b>	<b>148.5</b>	<b>117.5</b>	<b>163.5</b>	<b>73.5</b>	<b>679.0</b>	
Rail Right-of-Way	0.0	1.5	0.0	0.0	0.0	1.5	3.0	
Existing Highway 4 / 24 Ave R/W	5.5	4.5	0.0	0.0	0.0	0.0	10.0	
Future Arterial Roadways <sup>1</sup>	6	10.5	0	9	14	0	39.5	
Irrigation R/W	1.5	3.5	0	2	0	0	7.0	
Gas Transmission Pipeline R/W	1	0	1	0	1	0	3.0	
Undevelopable Area	0	0	0	0	8	0	8.0	
<b>GROSS DEVELOPABLE AREA</b>	<b>96.0</b>	<b>46.0</b>	<b>147.5</b>	<b>106.5</b>	<b>140.5</b>	<b>72.0</b>	<b>608.5</b>	<b>100%</b>
Local Roadways (Est. 20% GDA of non-semi-urban land)	19	9	29.5	21.5	28	0	107	20%
Collector Roadways	8	3.5	16	12.5	10.5	0	50.5	8%
Stormwater Management Facilities	6.5	4.5	11	11	11	0	44	7%
Park/School/Open Space (10% of GDA not including Outline Plan Area I) <sup>2</sup>	9.5	4.5	15	10.5	14	0	53.5	9%
	<b>43</b>	<b>21.5</b>	<b>71.5</b>	<b>55.5</b>	<b>63.5</b>	<b>0</b>	<b>255</b>	<b>42%</b>
Semi-Rural	0	0	0	0	0	72	72	12%
Retail Commercial	4	4	2	5	2	0	17	3%
Public Use (Religious Assembly, Recreation Facilities etc.) <sup>3</sup>	8	0	2	10	0	0	20	3%
Residential	8	0	72	36	75	0	191	31%
Business / Light Industrial/Office	33	20.5	0	0	0	0	53.5	9%
<b>NET DEVELOPABLE AREA</b>	<b>53</b>	<b>24.5</b>	<b>76</b>	<b>51</b>	<b>77</b>	<b>72</b>	<b>353.5</b>	<b>58%</b>
<b>Residential Units and Population</b>								
Low Density Area (ha)	0.0	0.0	62.5	31.5	65.5	0.0	159.5	
Low Density Dwelling Units	0	0	1375	693	1441	0	3509	
Low Density Population	0	0	3988	2010	4179	0	10177	
Units/Net Hectare	22.0	0.0	62.5	31.5	65.5	0.0	159.5	
People per Dwelling Unit	2.9	0	3988	2010	4179	0	10177	
Medium / High Density Area (ha)	8.0	0.0	9.5	4.5	10.0	0.0	32.0	
Medium / High Density Dwelling Units	560	0	665	315	700	0	2240	
Medium / High Density Population	1064	0	1264	599	1330	0	4257	
Units/Net Hectare	70.0	0.0	9.5	4.5	10.0	0.0	32.0	
People per Dwelling Unit	1.9	0	1264	599	1330	0	4257	
Total Area (ha)	8.0	0.0	72.0	36.0	75.5	0.0	191.5	
Total Dwelling Units	560	0	2040	1008	2141	0	5749	
Total Population	1064	0	5252	2609	5509	0	14434	
<b>School Generation</b>								
Total	71	0	349	173	366	0	889	
elementary age (5-10) % of total pop	6.7%	0	349	173	366	0	889	
middle school age (11-13) of total pop	3.0%	0	155	77	163	0	396	
High School Age (14-17) of total pop	4.1%	0	215	107	226	0	548	
Public District	47	0	231	115	242	0	587	
elementary age	66.0%	0	103	51	108	0	261	
middle school age	66.0%	0	142	71	149	0	362	
High School Age	66.0%	0	142	71	149	0	362	
Separate District	14	0	67	33	70	0	170	
elementary age	29.0%	0	30	15	31	0	76	
middle school age	29.0%	0	41	20	43	0	105	
High School Age	29.0%	0	41	20	43	0	105	
Alternative (Private School/Home Schooled)	1	0	3	2	4	0	9	
elementary age	5.0%	0	1	1	2	0	4	
middle school age	5.0%	0	1	1	2	0	4	
High School Age	5.0%	0	2	1	2	0	5	

<sup>1</sup>Assumes average right-of-way width of 60 metres, as only land adjacent to development requires a 15 metre buffer. This figure is approximate and subject to change.

<sup>2</sup> Park/School/Open Space has not been taken in Outline Plan Area I north of the railway, as development in this area will be minimal and might not be subdivided.

<sup>3</sup>Includes 2 religious assembly sites and a 8.2 hectare recreational facility site in Section 22.

**Notes to Reader:**

\* Land Uses shown are hypothetical and based upon what could occur in the Land Use Concept that is described in section 4 of this ASP. If land use concept changes at Outline Plan stage, an amendment to this Area Structure Plan is not required.

\* Additional area may need to be provided to maintain the irrigation district facilities in the area. This exact area will be determined at later planning stages when it is known what size of irrigation right-of-ways are required. This is also largely based upon the needs of the developer.

\* Figures are approximate and are rounded to the nearest 0.5 hectare.



## Appendix B – Recommended Land Use Districts



Recommended Land Use Districts (Based on Land Use Bylaw #5700 – February 2016)		
Land Use Shown in ASP	Potential Land Use Districts based on the Outline Plan	Land Use Districts Not Suitable
<i>Residential</i>	R-CL Comprehensively Planned Low Density Residential R-CM Comprehensively Planned Medium Density Residential R-L Low Density Residential R-M Mixed Density Residential R-37 Medium Density Residential R-50 Medium Density Residential R-60 Medium Density Residential R-75 Medium Density Residential R-100 Medium Density Residential R-150 Medium Density Residential P-B Public Building P-R Park and Recreation	R-RL Restricted Low Density Residential R-200 Medium Density Residential
<i>Community Nodes</i>	C-L Local Commercial C-H Highway Commercial C-N Neighbourhood Commercial R-CM Comprehensively Planned Medium Density Residential R-M Mixed Density Residential R-37 Medium Density Residential R-50 Medium Density Residential R-60 Medium Density Residential R-75 Medium Density Residential R-100 Medium Density Residential R-150 Medium Density Residential R-200 Medium Density Residential P-B Public Building P-R Park and Recreation	C-G General Commercial R-CL Comprehensively Planned Low Density Residential R-L Low Density Residential R-SL Small Parcel Low Density Residential Industrial Districts
<i>Special Planning Area</i>	C-G General Commercial C-H Highway Commercial I-B Business Industrial P-B Public Building P-R Park and Recreation R-37 Medium Density Residential R-50 Medium Density Residential R-60 Medium Density Residential R-75 Medium Density Residential R-100 Medium Density Residential R-150 Medium Density Residential R-CL Comprehensively Planned Low Density Residential R-CM Comprehensively Planned Medium Density Residential	I-H Heavy Industrial

**Recommended Land Use Districts (Based on Land Use Bylaw #5700 – Oct. 2015)**

<i>Special Planning Area</i>	R-L Low Density Residential R-M Mixed Density Residential	
<i>Semi-Rural</i>	FUD Future Urban Development P-T Transportation DC Direct Control	All other land use districts
<i>Schools</i>	P-B Public Building P-R Park and Recreation	All other land use districts
<i>Gateway Features</i>	P-B Public Building P-R Park and Recreation C-H Highway Commercial	All other land use districts

- Direct Control (DC) districts are possible throughout the entire plan area, so long as their intended use remains the same as what is presented in the ASP and the recommendations provided in this table, and where approved by the permitting authority.
- Public, recreational and institutional uses and their associated land use districts are allowed throughout the entire plan area where appropriate and approved by the permitting authority.
- New land use districts that do not yet exist in the Land Use Bylaw as October, 2015, but may exist in the future, will be evaluated for suitability in the SEASP area as they are created and applied.

Appendix C – Terms of Reference, Municipal Planning Commission  
Recommendation and Plan Review Committee Recommendation





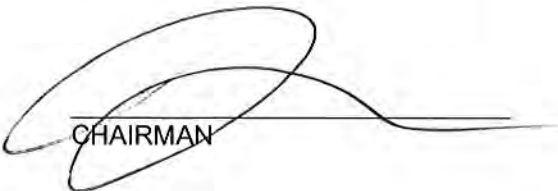
MOVED BY: LAN MacLACHLAN 

THAT the following amendments be made to the proposed Terms of Reference for the Southeast Area Structure Plan:

- Goal 1, Bullet 2 - to read as follows, "to comply with the Integrated Community Sustainability Plan / Municipal Development Plan (ICSP/MDP) Policy 6.4.2.1 in regards to user hierarchy"
- Goal 2, Bullet 1 - delete
- Goal 2, Bullet 2 - delete
- Goal 6, Bullet 1 - to read as follows, "locate medium and higher density forms of housing in neighbourhood nodes, which will comply with ICSP/MDP Policy 6.4.2.2"
- Planning Processes – second sentence to read as follows, "This process includes three public open houses as well as informal communication between City staff, landowners and/or their designates as required"

AND FURTHER THAT the applicant proceed with the planning process for the Southeast Area Structure Plan as:

- The amended Terms of Reference will allow the Southeast Area Structure Plan to meet the requirements stipulated in the *Municipal Government Act*
- The amended Terms of Reference are consistent with the policies of the ICSP/MDP
- The amended Terms of Reference will allow the Southeast Area Structure Plan to follow policy direction established under the Master Plans that have been adopted by Lethbridge City Council.

  
CHAIRMAN



## City of Lethbridge: Southeast Area Structure Plan Terms of Reference

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### Background

In compliance with the Integrated Community Sustainability Plan/Municipal Development Plan (ICSP/MDP), The City of Lethbridge will prepare an Area Structure Plan (ASP) prior to the subsequent subdivision and development of approximately 655 hectares (see Figure 1 & 2 for the ASP boundaries) of land located within South Lethbridge. A legal description and street address of the 55 different landowner's parcels which comprise the ASP can be found in Appendix A.

### Purpose

The Southeast ASP will set a policy and technical framework for more detailed planning that will take place in future outline plans, subdivision applications and eventually development permits.

### Policy Context

This ASP shall be prepared in accordance with the provisions stipulated in Section 633 of the *Municipal Government Act*. The requirements of the act are as follows:

- the sequence of development proposed for the area;
- the land uses proposed for an area, either generally or with respect to specific parts of the area;
- the density of population proposed for an area either generally or with respect to specific parts of the area;
- the general location of major transportation routes and public utilities; and
- any other matters Council considers necessary.

The City of Lethbridge Integrated Community Sustainability Plan/Municipal Development Plan (ICSP/MDP) will also play an integral role in guiding the policy development of this Plan. The ICSP/MDP is a holistic document and will guide policy development with regards to land use, infrastructure, transportation network, parks and open space, as well as community design. Specific reference to the ICSP/MDP will be required as part of this ASP.

Other important policy documents that will need to inform the policy development of the ASP are listed below:

- Neighbouring ASP's such as Arbour Ridge, Fairmont Park, and Southgate will need to be examined to ensure that any interface issues are dealt with in an appropriate manner.
- The current Transportation Master Plan
- Parks Master Plan
- Bikeways and Pathways Master Plan
- Intermunicipal Development Plan
- Recreation and Culture Master Plan
- Any plans that have been adopted for the adjacent lands within the County of Lethbridge will need to be reviewed.

## **Development Area**

Within this component of the ASP a table documenting the property owner, the legal land description, and the total number of hectares will be included. As per the ASP Terms of Reference Guidelines, copies of all titles will be appended to the ASP, and within the document there will be a section that addresses the implications of any restrictive covenants.

The ASP will also consider the regional context in its approach. It will identify any constraints (e.g. gas wells, confined feeding operations etc.) that have the potential to directly impact future development in the plan area, but exist outside the plan area within a radius of 3.2 kilometres (2 miles). Within the development area of the plan there are a number of maps and associated textual sections that need to be included within the ASP. They are as follows:

- A map showing the regional context.
- A contour map showing the highest resolution of the contours readily available.
- A natural features map: This map will illustrate any natural drainage areas, wetlands, significant vegetation, steep slopes etc. The textual review associated with this map will focus on any important findings from the essential studies or reports which identified these important natural features.
- Physical constraints map: This map will need to show known constraints such as the main irrigation canal, and the overhead electric transmission lines. Findings from the essential studies and engineering reports that are required for all ASP's must also be shown on this map. A textual review and summary explaining the map will form part of this section of the ASP.

A map showing the ASP boundaries will also be included. However, for the purposes of this Terms of Reference, an ASP Boundary Map has been included and can be found on the following page as Figure 2.

**Figure 1 Southeast Area Structure Plan Landowners**

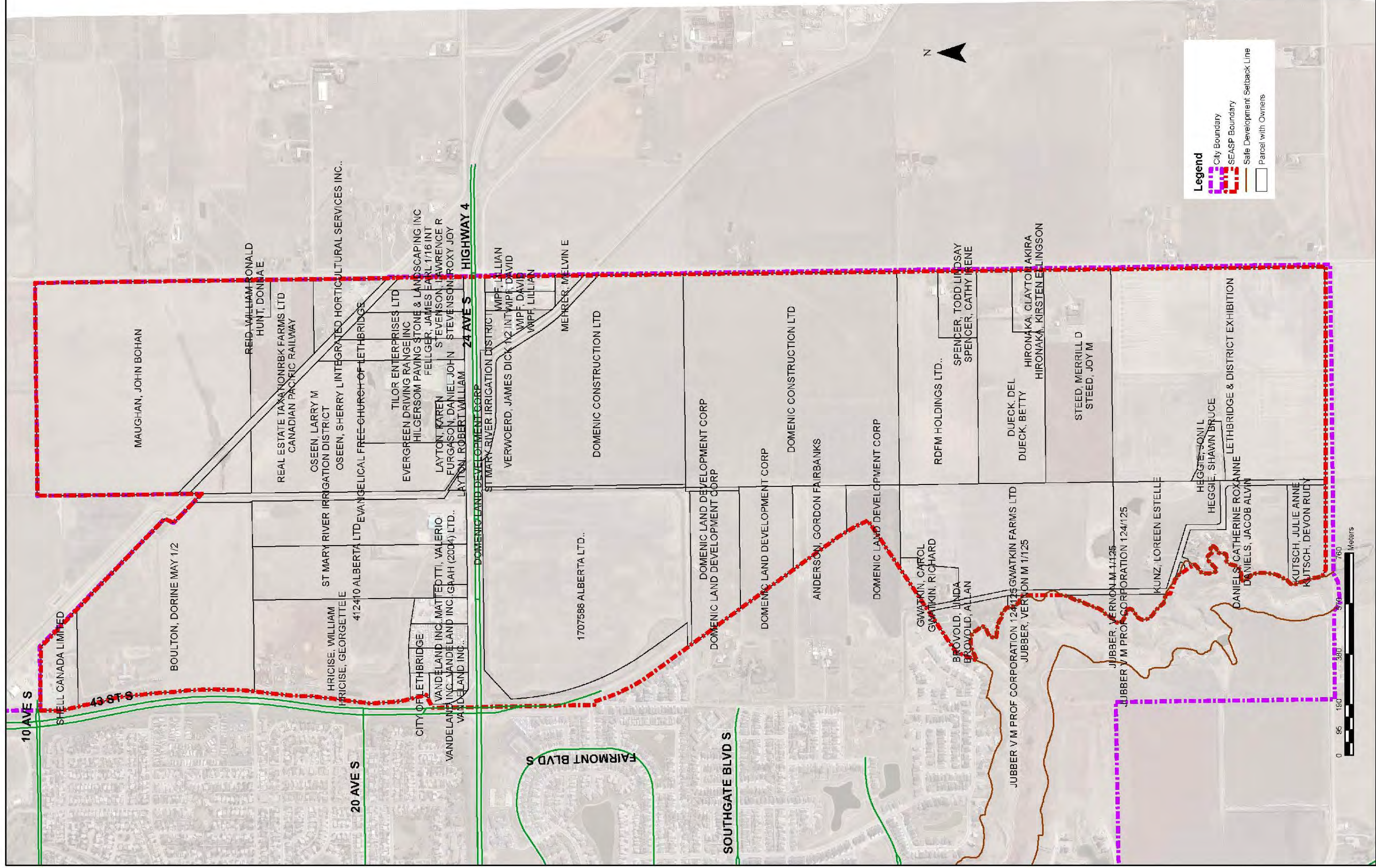
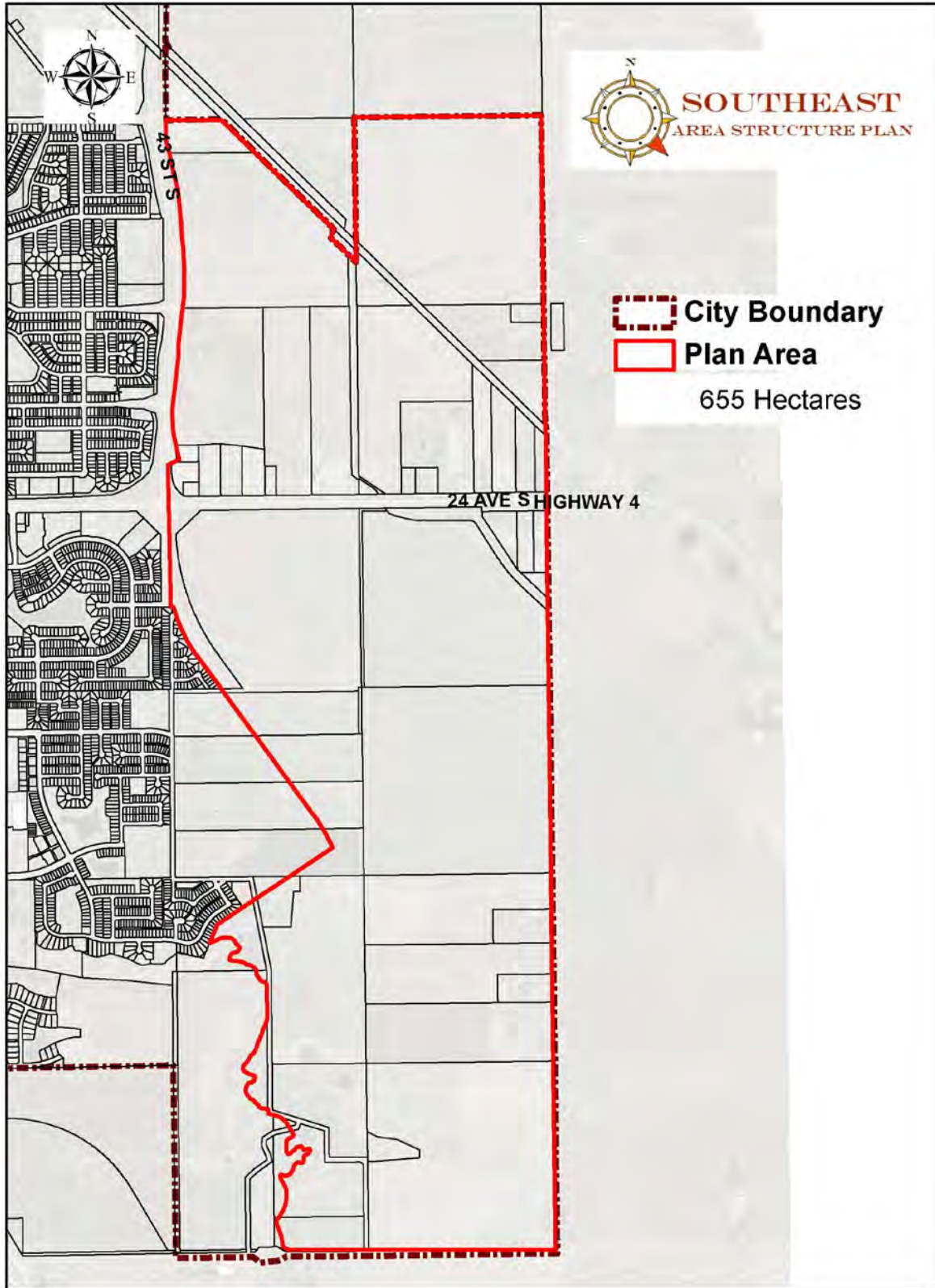


Figure 2



## **Development Plan**

Within this component of the ASP, the strategy for development will be articulated. An overall vision of the area will also be established to help set the tone for the ASP goals, objectives and most importantly the eventual policies.

### **Vision:**

To develop an ASP which integrates natural elements of the Six-Mile Coulee and a built environment that supports the needs of the residents of Southeast Lethbridge, and the City as a whole. It will also create a distinctive sense of place through its use of a multi-modal and connective transportation network.

### **Goals and Objectives:**

#### **Goal 1**

To design a community that accommodates all forms of travel.

##### Objectives

- Transition appropriately from a curvilinear design to a grid based pattern for the neighbourhood design.
- Accommodate pedestrian, cyclist, and vehicular movement throughout the community to comply with the Integrated Community Sustainability Plan / Municipal Development Plan (ICSP/MDP) Policy 6.4.2.1 in regards to user hierarchy.
- In order to maximize the benefit of a grid based system use an extensive collector network.
- Design the transportation network with the intent of moving people and goods and creating multiple routes to destinations.
- Where appropriate, utilize innovative road configurations within the plan (e.g. roundabouts, one way streets, narrow carriage ways and zero curb line).
- Ensure appropriate access for commercial and industrial land use.

#### **Goal 2**

To develop a gateway corridor into the City that fosters a sense of place and incorporates urban design criteria for public and private spaces.

##### Objectives

- Provide public realm improvements (i.e. street lights, benches, garbage receptacles, landscaping and street trees) where appropriate that are consistent throughout the corridor.

- Consider a new route or design for the existing overhead electric transmission lines.
- The corridor needs to address transitioning to adjacent uses (i.e. industrial and residential)

### **Goal 3**

To create a connected open space and park system that includes Six-Mile Coulee.

#### Objectives

- Protect public access to Six-Mile Coulee by creating a public open space system along the top of bank.
- Have a pathway system that connects the natural Six-Mile Coulee public open space system to the urban environment
- Provide regional pathway connections.

### **Goal 4**

To provide public use node(s) that will accommodate a range of programs and uses.

#### Objectives

- Provide sufficient land for a recreational facility.
- Within the public use node(s), ensure that lands are designated for a transit hub, recycling facility and protective services.
- Integrate the node(s) with Institutional uses such as schools, senior facilities and religious assemblies
- Ensure public use node(s) are accessible by a variety of transportation modes.

### **Goal 5**

To provide an appropriate mix of land uses which may include: residential, commercial, recreational, institutional, general industrial and business industrial.

#### Objectives

- Ensure that land uses are interfaced appropriately, and that they relate effectively with the grid based transportation network.
- Create neighbourhood nodes which will be focused around mixed use development.
- Ensure that the existing neighbourhoods of Fairmont and South Gate are interfaced appropriately, and their neighbourhood edges are considered in the design process.



## **Goal 6**

To develop a community where concentrations of increased residential density can support commercial, recreational and institutional land uses.

### Objectives

- Locate medium and higher density forms of housing in neighbourhood nodes, which will comply with ICSP/MDP Policy 6.4.2.2.
- Provide appropriate infrastructure to nodal areas, to ensure that increased residential density can occur.

## **Goal 7**

To develop neighbourhoods which are distinct in character, yet are interconnected in function.

### Objectives

- Explore opportunities for multi-functional public uses.
- Connect neighbourhoods by providing multiple access points between neighbourhoods, where possible.
- Distinguish neighbourhoods by focal points and design elements.

## **Goal 8**

To plan in manner where the urban/rural interface is considered and designed to benefit both the City and the County.

### Objectives

- Involve the County as a key stakeholder throughout the planning process.
- Review any plans that have been adopted for the adjacent lands within the County of Lethbridge.
- Ensure that land use and servicing concepts are complimentary and appropriate.

## **Goal 9**

Throughout the planning process seek to ensure that ongoing financial efficiencies are achieved.

### Objectives

- Examine the benefits of using a financial model to look at the true costs of development for this type of project.
- Ensure municipal infrastructure investments within the plan area are staged appropriately from a timing perspective, as this can minimize premature expenditures.

## **Land Use Concept**

Within this subsection of the Development Plan the Land Use Concept will be created. There are two essential components to this section. The first being a concept map, and the second being the associated textual overview which will include policies.

A map showing the concept will be included, and it will provide enough detail to allow for approximate land use calculations. The calculations must provide information with regards to the gross developable land, as well as, the net developable land; in the case that Environmental Reserve (ER) is taken. Calculations will be shown in hectares as per the stipulated requirements. To help encourage the more efficient use of land in Lethbridge and conform to the policy direction of the ICSP/MDP the density target for this Plan shall be greater than 18 net developable units per hectare, but less than 30 net developable units per hectare. A population projection must also be included in this section of the document. As part of this projection, school age population will also be included.

An overview of the Land Use Concept will need to form part of this section of the document, and policies will need to accompany each land use overview. The policies will need to derive direction from the vision, goals, and objectives as well as the findings from the essential environmental studies and engineering reports. Policies must also reference the policy context stipulated in this Terms of Reference.

A number of maps illustrating important concepts will be included in this subsection. General textual reviews will need to accompany the maps. The following maps and associated textual reviews are listed below:

1. Servicing and utilities – a series of maps showing general location of mains and other infrastructure such as storm water ponds.
  - A storm water concept map
  - A water concept map
  - A sanitary sewer concept map
  - A fire response modeling map
2. Transportation network concept map - Staging of the system for neighbourhood development will be illustrated. The Arterial and collector network will also be shown at this level of planning. Transit information will be included.

3. Schools, park and open space concept map – General locations and sequence of development will be addressed.
4. Phasing concept map – This map and textual review will detail the logical outline plan boundaries, and in what sequence of development these plans should take place.

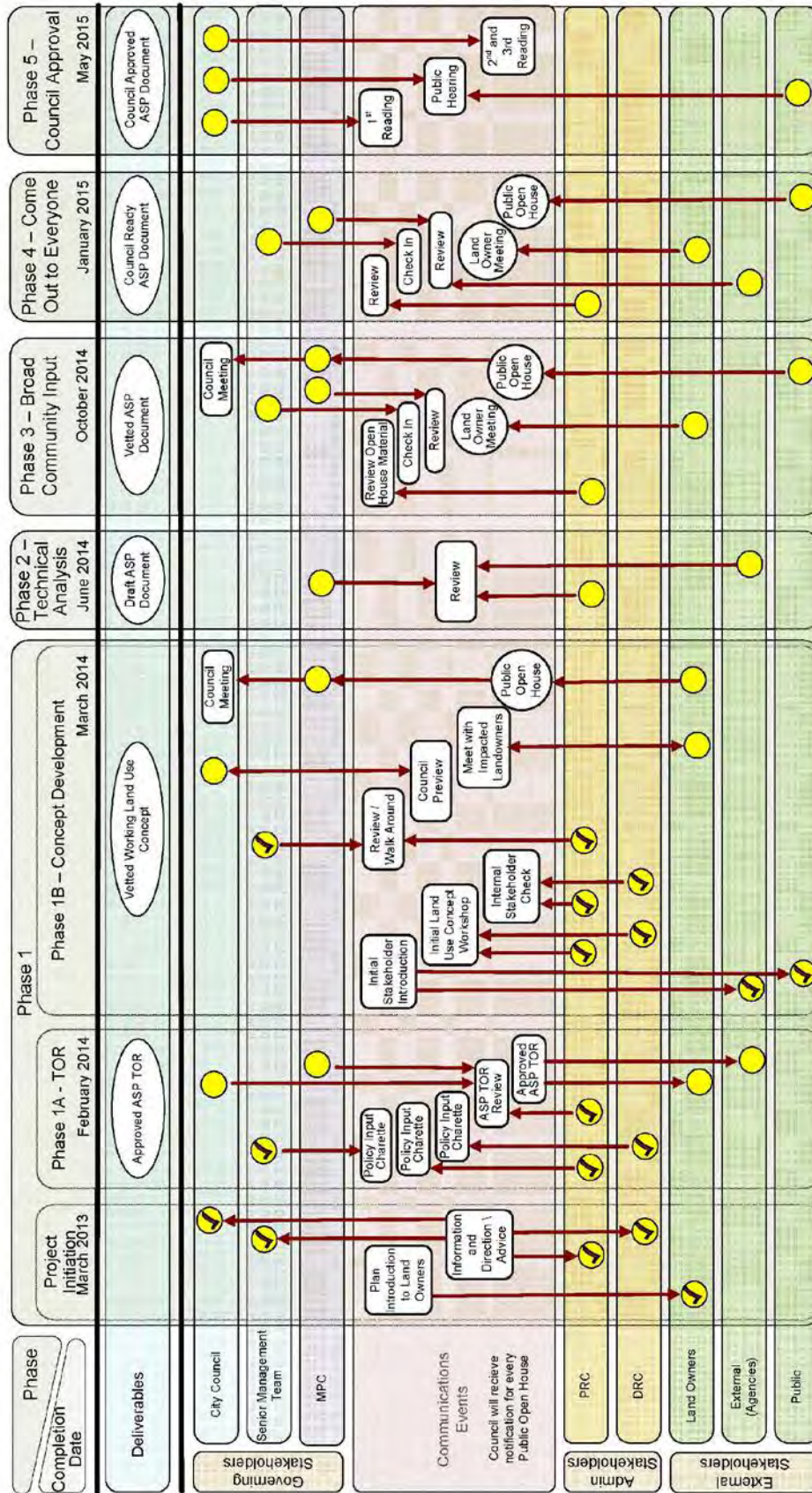
### **Planning Process**

The proposed planning process is outlined on Figure 3 on the next page. This process includes three public open houses, as well as informal communication between City staff, landowners and/or their designates as required. The final draft of the ASP will take comments received from the public, stakeholders and area landowners into consideration and will be presented at the final open house prior to reading by City Council.

### **Reports and Studies**

Engineering and environmental details will be appended to the ASP under separate cover reports and studies.

Figure 3 – Proposed Planning Process



## Appendix A

Listed below by civic address, legal description and ownership are the 55 parcels which comprise the ASP:

<b>Civic Address</b>	<b>Legal Address</b>	<b>Ownership</b>
5425 24 AVE S	PLAN 1546GQ BLOCK B	HILGERSOM PAVING STONE & LANDSCAPING INC
5605 24 AVE S	PLAN 5985GM THAT PORTION OF PARCEL A, WHICH LIES TO THE WEST OF THE EASTERLY ONE HUNDRED AND SIXTY FOUR (164) FEET THROUGHOUT THE SAID PARCEL A CONTAINING 2.729 HECTARES (6.74 ACRES) MORE OR LESS	FELLGER, JAMES EARL 1/16 INT
5725 24 AVE S	PLAN 5985GM PARCEL A THE EASTERLY 164 FEET IN PERPENDICULAR WIDTH THROUGHOUT OF THAT PORTION OF THE SOUTH EAST QUARTER OF SECTION 27 IN TOWNSHIP 8 RANGE 21 MERIDIAN 4 CONTAINING 3.24 ACRES MORE OR LESS	STEVENSON, LAWRENCE R & STEVENSON, ROXY JOY
5035 24 AVE S	PLAN 7610425 BLOCK E CONTAINING 23.6 HECTARES (58.41 ACRES) MORE OR LESS EXCEPTING THEREOUT: PLAN NUMBER HECTARES (ACRES) MORE OR LESS CANAL R/W 0312659 0.378 0.93	OSEEN, SHERRY L & OSEEN, LARRY M
5315 24 AVE S	PLAN 1546GQ BLOCK C	TILOR ENTERPRISES LTD
2002 58 ST S	PLAN 367GJ PARCEL "A" CONTAINING TEN (10) ACRES (4.05 HECTARES) MORE OR LESS EXCEPTING THEREOUT: ROAD PLAN 9111978 CONTAINING 0.146 HECTARES (0.361 ACRES) MORE OR LESS	INTEGRATED HORTICULTURAL SERVICES INC..
1602 58 ST S	THE NORTH EAST QUARTER OF SECTION TWENTY SEVEN (27) IN TOWNSHIP EIGHT (8) RANGE TWENTY ONE (21) WEST OF THE FOURTH MERIDIAN CONTAINING ONE HUNDRED AND SIXTY (160) ACRES MORE OR LESS EXCEPTING THEREOUT PLAN NUMBER HECTARES (ACRES) RAILWA	MAUGHAN, JOHN BOHAN
1802 58 ST S	PLAN 0410527 BLOCK 1 LOT 1  2.75 ACRES	HUNT, DONNA E & REID, WILLIAM RONALD

1850 58 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 27 THAT PORTION OF THE SOUTH EAST QUARTER WHICH LIES NORTHEAST OF RAILWAY ON PLAN RY23 CONTAINING 13.4 HECTARES (33.1 ACRES) MORE OR LESS EXCEPTING THEREOUT: PLAN NUMBER HECTARES (ACRES) MORE O	RBK FARMS LTD
709 1 AVE S	PLAN MAIN LINE, 8.705 MILES  RIGHT OF WAY IN SEC1-9-22-4,2-9-22-4,3-9-22-4,6-9-21-4,35-8- 22-4,36-8-22-4,33-8-21-4 & PLAN 8410666 BLK A & PLAN 8711578 BLK 2 & 5	CANADIAN PACIFIC RAILWAY REAL ESTATE TAXATION
4025 43 ST S	PLAN 0111356 BLOCK 2 LOT 1  (8.43 ACRES)	GWATKIN, RICHARD & GWATKIN, CAROL
5310 24 AVE S	PLAN 1457JK LOT 1	VERWOERD, JAMES DICK 1/2 INT
5620 24 AVE S	PLAN 1457JK LOT 2	WIPF, DAVID & WIPF, LILLIAN
4310 24 AVE S	DESCRIPTIVE PLAN 0914974 BLOCK 12 LOT 10	1707588 ALBERTA LTD..
5730 24 AVE S	PLAN 1457JK LOT 3	WIPF, DAVID & WIPF, LILLIAN
4820 58 ST S	THE NORTH EAST QUARTER OF SECTION FIFTEEN (15), IN TOWNSHIP EIGHT (8) RANGE TWENTY ONE (21) WEST OF THE FOURTH MERIDIAN, CONTAINING ONE HUNDRED AND SIXTY (160) ACRES, MORE OR LESS EXCEPTING: FIRSTLY: THE NORTHERLY SEVEN HUNDRED AND NINETY TWO (792) FEET	STEED, MERRILL D & STEED, JOY M
5503 43 ST S	PLAN 0310216 BLOCK 1 LOT 2  4.67 ACRES	HEGGIE, SHAWN BRUCE & HEGGIE, JONI L
4406 58 ST S	PLAN 9910301 BLOCK 1  9.19 ACRES MORE OR LESS	SPENCER, TODD LINDSAY & SPENCER, CATHY IRENE

3010 58 ST S	PLAN: 22-8-21-4 BLOCK: NE LOT:  160 ACS EXC: IRR189(6.60),3371EZ(7.38),  761JK(.57),1457JK(16.75);  PTN OF N 172' S OF HWY R/W  3371EZ & W OF R/W 761JK(0.36 ACS);  PTN OF W 40' S OF N 172'(2.27 ACS);  PTN CANAL R/W 0311792 SO OF R/W IRR & S OF S BD	DOMENIC CONSTRUCTION LTD
4446 58 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 THE NORTHERLY 792 FEET OF THE SOUTHERLY 1658.25 FEET OF THE NORTH EAST QUARTER CONTAINING 19.4 HECTARES (48 ACRES) MORE OR LESS EXCEPTING THEREOUT: PLAN NUMBER HECTARES ACRES MORE OR LESS SUBDI	DUECK, DEL & DUECK, BETTY
5415 43 ST S	PLAN 0310216 BLOCK 1 LOT 3	LETHBRIDGE & DISTRICT EXHIBITION
5205 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 THAT PORTION OF LEGAL SUBDIVISION 6 IN THE SOUTH WEST QUARTER WHICH LIES TO THE NORTH AND EAST OF THE ROADWAY ON PLAN 1025EZ CONTAINING 12.1 HECTARES (30 ACRES) MORE OR LESS	KUNZ, LOREEN ESTELLE
3927 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 THE SOUTH HALF OF LEGAL SUBDIVISIONS 3 AND 4 IN THE SOUTH WEST QUARTER CONTAINING 16.2 HECTARES (40 ACRES) MORE OR LESS	DOMENIC LAND DEVELOPMENT CORP
2820 58 ST S	PLAN 1457JK LOT 4 (RESERVE)	MEHRER, MELVIN E
3605 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 THE NORTH HALF OF LEGAL SUBDIVISIONS 3 AND 4 IN THE SOUTH WEST QUARTER CONTAINING 16.2 HECTARES (40 ACRES) MORE OR LESS	ANDERSON, GORDON FAIRBANKS
3115 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 THE NORTH HALF OF LEGAL SUBDIVISIONS 5 AND 6 IN THE SOUTH WEST QUARTER CONTAINING 16.2 HECTARES (40 ACRES) MORE OR LESS EXCEPTING THEREOUT THE NORTHERLY 50 FEET THROUGHOUT SAID LEGAL SUBDIVISIONS, AND THAT PORTIO	DOMENIC LAND DEVELOPMENT CORP

4215 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 THAT PORTION OF THE NORTH WEST QUARTER WHICH LIES EAST OF ROAD PLAN 3187K CONTAINING 33.8 HECTARES (83.52 ACRES) MORE OR LESS EXCEPTING THEREOUT: PLAN NUMBER HECTARES (ACRES) MORE OR LESS SU	GWATKIN FARMS LTD
3125 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 THE NORTHERLY 50 FEET THROUGHOUT LEGAL SUBDIVISIONS 5 AND 6 AND THAT PORTION OF THE EASTERLY 50 FEET OF THE NORTH HALF OF LEGAL SUBDIVISION 6 WHICH LIES TO THE SOUTH OF THE NORTHERLY 50 FEET OF THE SAID LEGAL SUB	DOMENIC LAND DEVELOPMENT CORP
4610 58 ST S	PLAN 0613929 BLOCK 1 LOT 3	HIRONAKA, CLAYTON AKIRA & HIRONAKA, KIRSTEN ELLINGSON
4304 58 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 THAT PORTION OF THE NORTH EAST QUARTER WHICH LIES NORTH OF THE SOUTHERLY 1658.25 FEET THROUGHOUT OF SAID QUARTER SECTION CONTAINING 23.92 HECTARES (59.5 ACRES) MORE OR LESS EXCEPTING THEREOUT: PLAN NUMB	RDFM HOLDINGS LTD..
3110 58 ST S	PLAN NOPLAN MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 QUARTER SE	DOMENIC CONSTRUCTION LTD
4631 24 AVE S	PLAN 9811073 BLOCK 6	GAAH (2004) LTD..
4651 24 AVE S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 27 THE WEST HALF OF LEGAL SUBDIVISIONS 3 AND 6 OF THE SOUTH WEST QUARTER CONTAINING 16.2 HECTARES (40 ACRES) MORE OR LESS EXCEPTING THEREOUT: THAT PORTION LYING BETWEEN TWO LINES DRAWN PARALLEL WITH AND 30 FEET PE	412410 ALBERTA LTD
4607 24 AVE S	PLAN 7452GK BLOCK 5	MATTEOTTI, VALERIO
4901 24 AVE S	PLAN 0411833 BLOCK 1 LOT 2  0.8 ACS	APC HOLDINGS INC

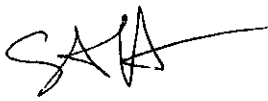


4905 24 AVE S	PLAN 0312659 CANAL RIGHT OF WAY IN THE SOUTH EAST QUARTER OF SECTION 27 IN TOWNSHIP 8 RANGE 21 WEST OF THE FOURTH MERIDIAN CONTAINING 0.513 HECTARES (1.27 ACRES) MORE OR LESS EXCEPTING THEREOUT: A) THAT PORTION OF SAID CANAL RIGHT OF WAY WHICH LIES WITHI	ST MARY RIVER IRRIGATION DISTRICT
2275 43 ST S	PLAN 8611028 SUB-STATION SITE CONTAINING 0.704 HECTARE (1.74 ACRES) MORE OR LESS	CITY OF LETHBRIDGE
1005 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 27 THAT PORTION OF THE NORTH 463.2 FEET OF THE NORTH WEST QUARTER WHICH LIES TO THE WEST OF THE RAILWAY ON PLAN RY23 AND TO THE EAST OF THE ROAD WIDENING ON PLAN 7911470 CONTAINING 4.204 HECTARES (10.38 ACRES) MORE	SHELL CANADA LIMITED
1125 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 27 THAT PORTION OF THE NORTH WEST QUARTER WHICH LIES TO THE SOUTH WEST OF THE RAILWAY ON PLAN RY23 AND TO THE SOUTH OF THE NORTHERLY 463.2 FEET OF THE SAID QUARTER SECTION, CONTAINING 45.1 HECTARES (111.69 ACRES) MO	BOULTON, DORINE MAY 1/2
1625 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 27 LEGAL SUBDIVISIONS 4 AND 5 IN THE SOUTH WEST QUARTER CONTAINING 32.4 HECTARES (80 ACRES) MORE OR LESS EXCEPTING THEREOUT PLAN NUMBER HECTARES ACRES MORE OR LESS PARCEL A 6	HRICISE, WILLIAM & HRICISE, GEORGETTE E
4403 24 AVE S	PLAN 7452GK BLOCK 3	VANDELAND INC..
4505 24 AVE S	PLAN 7452GK BLOCK 4	VANDELAND INC..
5225 24 AVE S	PLAN 9912246 BLOCK D LOT 3	EVERGREEN DRIVING RANGE INC
5105 24 AVE S	PLAN 9912246 BLOCK D LOT 5	LAYTON, ROBERT WILLIAM & LAYTON, KAREN
5115 24 AVE S	PLAN 9912246 BLOCK D LOT 4	FURGASON, DANIEL JOHN
4301 24 AVE S	PLAN 0312980 BLOCK 2 LOT 2  1.04 ACRES	VANDELAND INC..

2325 43 ST S	PLAN 0312980 BLOCK 2 LOT 1  2.84 ACRES	VANDELAND INC..
4717 24 AVE S	DESCRIPTIVE PLAN 1111181 BLOCK 7 LOT 7	EVANGELICAL FREE CHURCH OF LETHBRIDGE
3325 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 THE SOUTH HALF OF LEGAL SUBDIVISIONS 5 AND 6 IN THE SOUTH WEST QUARTER CONTAINING 16.2 HECTARES (40 ACRES) MORE OR LESS	DOMENIC LAND DEVELOPMENT CORP
3000 58 ST S	PLAN 0311792 CANAL RIGHT OF WAY WITHIN MERIDIAN 4, RANGE 21, TOWNSHIP 8 SECTION 22 QUARTER NORTH EAST THAT PORTION LYING SOUTH OF THE SOUTHERLY BOUNDARY OF RIGHT OF WAY PLAN IRR 189 AND SOUTH OF THE SOUTHERLY BOUNDARY OF RIGHT OF WAY PLAN 761 JK EXCEPTIN	ST MARY RIVER IRRIGATION DISTRICT
4900 24 AVE S	PLAN NOPLAN MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22  FIRSTLY: THAT PORTION OF THE NORTHERLY 172 FEET OF THE NORTH EAST QUARTER WHICH LIES SOUTH OF THE HIGHWAY RIGHT OF WAY ON PLAN 3371EZ AND WEST OF THE CANAL RIGHT OF WAY ON PLAN 761JK, CONTAINING .146	DOMENIC LAND DEVELOPMENT CORP
4220 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 THAT PORTION OF LEGAL SUBDIVISION 13 IN THE NORTH WEST QUARTER WHICH LIES TO THE SOUTH AND WEST OF THE ROADWAY ON PLAN 3187K CONTAINING 13.264 HECTARES (32.9) ACRES MORE OR LESS EXCEPTING THEREOUT: PLAN	BROVOLD, ALLAN & BROVOLD, LINDA
5920 43 ST S	PLAN 9210487 BLOCK 1 LOT 1	KUTSCH, DEVON RUDY & KUTSCH, JULIE ANNE
4045 60 AVE S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 LEGAL SUBDIVISIONS 4 AND 5 IN SOUTH WEST QUARTER EXCEPTING THEREOUT: FIRST: OUT OF LEGAL SUBDIVISION 4 THAT PORTION THEREOF WHICH LIES TO THE SOUTH OF A LINE DRAWN PARALLEL WITH 40 FEET PERPENDICULARLY DISTANT NO	JUBBER V M PROF CORPORATION 124/125 & JUBBER, VERNON M 1/125

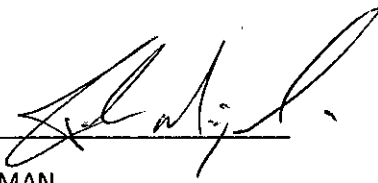
5310 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 LEGAL SUBDIVISION 3 AND LEGAL SUBDIVISION 6 IN THE SOUTH WEST QUARTER CONTAINING 32.4 HECTARES (80 ACRES) MORE OR LESS EXCEPTING THEREOUT: (A) PLAN NUMBER    HECTARES    ACRES ROAD DIVERSION	DANIELS, JACOB ALVIN & DANIELS, CATHERINE ROXANNE
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DATE: February 16, 2016

MOVED BY Stephen Neis 

That the Municipal Planning Commission recommends approval of the Southeast Area Structure Plan (ASP) by Lethbridge City Council as:

- The policy content and public consultation aspects of the Southeast ASP are consistent with the Terms of Reference that were approved by the Municipal Planning Commission on March 11, 2014.

  
\_\_\_\_\_  
CHAIRMAN

DATE: February 4, 2016

MOVED BY Darwin Juell

That the Plan Review Committee recommends approval of the Southeast Area Structure Plan (ASP) by Lethbridge City Council as:

- The Southeast ASP has been reviewed by City of Lethbridge Plan Review Committee and has satisfied the technical merits that are required in a functional Area Structure Plan.

  
PLAN REVIEW COMMITTEE CHAIRMAN



Appendix D – City-Wide High Intensity Residential Fire Analysis – As of  
September 2015





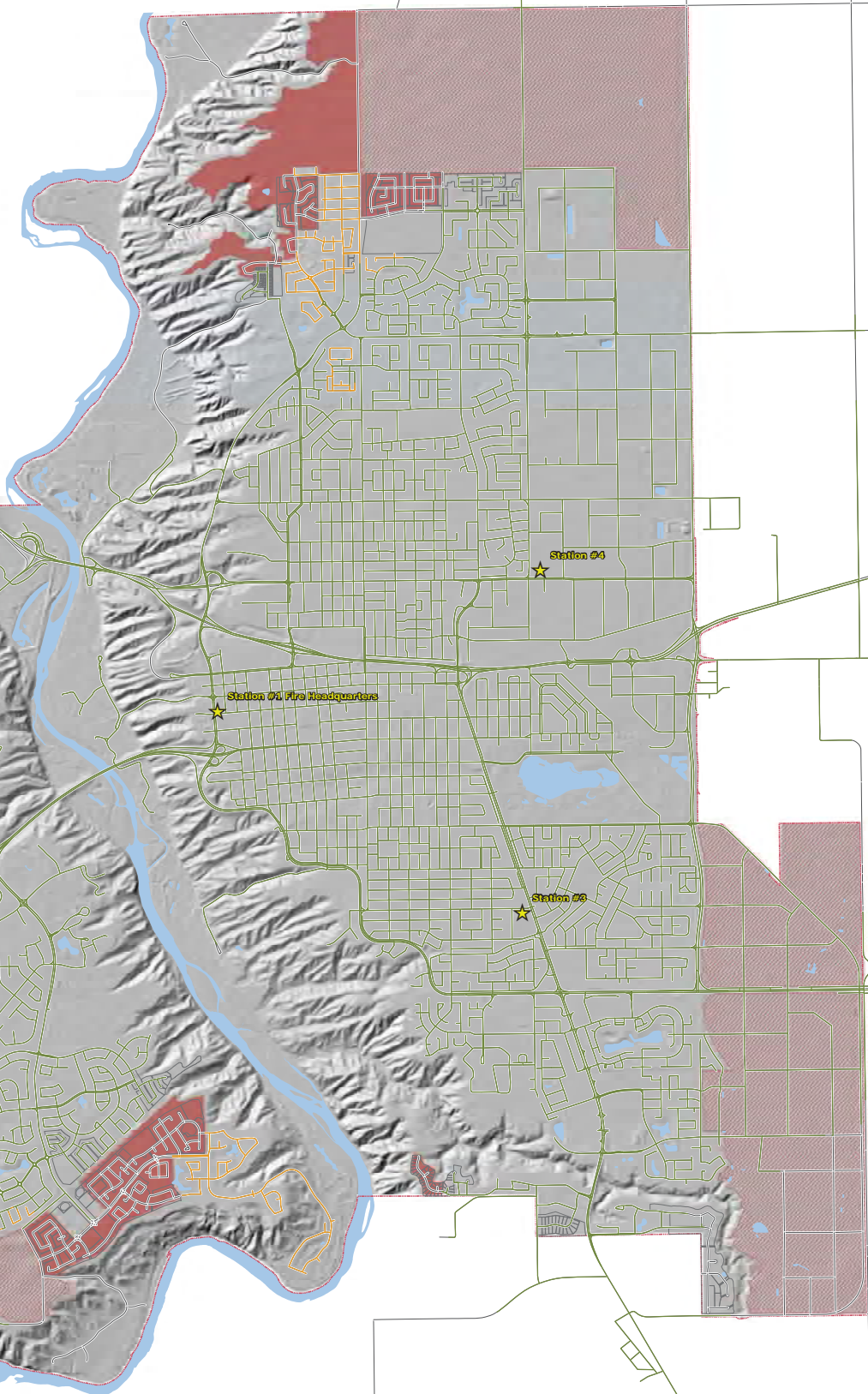


# ALBERTA BUILDING CODE INTERPRETATION HIGH INTENSITY RESIDENTIAL FIRE ANALYSIS

~ PLANNING & DEVELOPMENT DEPARTMENT ~  
SEPTEMBER 2015

## Legend

- ★ Fire Station
- 7 Minute Drive Time
- Road Segment Not Covered Within Response Time (HIRF Exempt)
- Road Segment Not Covered Within Response Time
- Areas Subject to HIRF Requirements
- Areas May Be Subject to HIRF Requirements (TBD)
- City Boundary
- Lake or River



### GIS Analysis

This analysis predicts emergency response times and geographic service areas for fire department units deployed from existing fire station locations in the City of Lethbridge.

Using ArcGIS 10.x and Network Analyst Geographic Information System (GIS) software, the Lethbridge street network was analyzed to generate predicted road coverage within a drive time of 7 minutes.

**The street network used in this analysis is based on September 2015 actual and outline plans information. This will change as accesses are provided and subdivisions are registered.**

Source of Information: City of Lethbridge Geographical Information System Data compiled from field survey data and historical records.

This map was produced by:  
Information Technology  
City of Lethbridge, AB Canada

Published September, 2015



## Appendix E – Area Landowners – As of December 2015



## SEASP Area Land Owners - December 2015

Civic Address	Legal Address	Land Owner 1	Land Owner 2
4631 24 AVE S	PLAN 9811073 BLOCK 6	GAAH (2004) LTD..	
4651 24 AVE S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 27 THE WEST HALF OF LEGAL SUBDIVISIONS 3 AND 6 OF THE SOUTH WEST QUARTER CONTAINING 16.2 HECTARES (40 ACRES) MORE OR LESS EXCEPTING THEREOUT: THAT PORTION LYING BETWEEN TWO LINES DRAWN PARALLEL WITH AND 30 FEET PE	412410 ALBERTA LTD.	
4607 24 AVE S	PLAN 7452GK BLOCK 5	MATTEOTTI, VALERIO	
4901 24 AVE S	PLAN 0411833 BLOCK 1 LOT 2 0.8 ACS	APC HOLDINGS INC	
5425 24 AVE S	PLAN 1546GQ BLOCK B	HILGERSOM PAVING STONE & LANDSCAPING INC	
5605 24 AVE S	PLAN 5985GM THAT PORTION OF PARCEL A, WHICH LIES TO THE WEST OF THE EASTERLY ONE HUNDRED AND SIXTY FOUR (164) FEET THROUGHOUT THE SAID PARCEL A CONTAINING 2.729 HECTARES (6.74 ACRES) MORE OR LESS	NEILSON, CAROLYN ROSALIE 1/8 INT	
5725 24 AVE S	PLAN 5985GM PARCEL A THE EASTERLY 164 FEET IN PERPENDICULAR WIDTH THROUGHOUT OF THAT PORTION OF THE SOUTH EAST QUARTER OF SECTION 27 IN TOWNSHIP 8 RANGE 21 MERIDIAN 4 CONTAINING 3.24 ACRES MORE OR LESS	BOSCH, LLOYD ALLAN	BOSCH, KERRY PATRICIA
4905 24 AVE S	PLAN 0312659 CANAL RIGHT OF WAY IN THE SOUTH EAST QUARTER OF SECTION 27 IN TOWNSHIP 8 RANGE 21 WEST OF THE FOURTH MERIDIAN CONTAINING 0.513 HECTARES (1.27 ACRES) MORE OR LESS EXCEPTING THEREOUT: A) THAT PORTION OF SAID CANAL RIGHT OF WAY WHICH LIES WITHI	ST MARY RIVER IRRIGATION DISTRICT	
2275 43 ST S	PLAN 8611028 SUB-STATION SITE CONTAINING 0.704 HECTARE (1.74 ACRES) MORE OR LESS	CITY OF LETHBRIDGE	
5035 24 AVE S	PLAN 7610425 BLOCK E CONTAINING 23.6 HECTARES (58.41 ACRES) MORE OR LESS EXCEPTING THEREOUT: PLAN NUMBER HECTARES (ACRES) MORE OR LESS CANAL R/W 0312659 0.378 0.93	OSEEN-THOMPSON, SHERRY LADENE 1/2 INT	
5315 24 AVE S	PLAN 1546GQ BLOCK C	TILOR ENTERPRISES LTD	
2002 58 ST S	PLAN 367GJ PARCEL ""A"" CONTAINING TEN (10) ACRES (4.05 HECTARES) MORE OR LESS EXCEPTING THEREOUT: ROAD PLAN 9111978 CONTAINING 0.146 HECTARES (0.361 ACRES) MORE OR LESS	INTEGRATED HORTICULTURAL SERVICES INC..	
1005 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 27 THAT PORTION OF THE NORTH 463.2 FEET OF THE NORTH WEST QUARTER WHICH LIES TO THE WEST OF THE RAILWAY ON PLAN RY23 AND TO THE EAST OF THE ROAD WIDENING ON PLAN 7911470 CONTAINING 4.204 HECTARES (10.38 ACRES) MORE	SFJ INC.	
1125 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 27 THAT PORTION OF THE NORTH WEST QUARTER WHICH LIES TO THE SOUTH WEST OF THE RAILWAY ON PLAN RY23 AND TO THE SOUTH OF THE NORTHERLY 463.2 FEET OF THE SAID QUARTER SECTION, CONTAINING 45.1 HECTARES (111.69 ACRES) MO	BOULTON, NORMA COLLEEN PTN OF 1/2 INT	
1625 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 27 LEGAL SUBDIVISIONS 4 AND 5 IN THE SOUTH WEST QUARTER CONTAINING 32.4 HECTARES (80 ACRES) MORE OR LESS EXCEPTING THEREOUT PLAN NUMBER HECTARES ACRES MORE OR LESS PARCEL A 6	ROMAN CATHOLIC BISHOP OF THE DIOCESE OF CALGARY.	
4403 24 AVE S	PLAN 7452GK BLOCK 3	VANDELAND INC..	
4505 24 AVE S	PLAN 7452GK BLOCK 4	VANDELAND INC..	
5225 24 AVE S	PLAN 9912246 BLOCK D LOT 3	EVERGREEN DRIVING RANGE INC	
5105 24 AVE S	PLAN 9912246 BLOCK D LOT 5	LAYTON, KAREN	
5115 24 AVE S	PLAN 9912246 BLOCK D LOT 4	FURGASON, DANIEL JOHN	
4301 24 AVE S	PLAN 0312980 BLOCK 2 LOT 2 1.04 ACRES	VANDELAND INC..	
2325 43 ST S	PLAN 0312980 BLOCK 2 LOT 1 2.84 ACRES	VANDELAND INC..	
1802 58 ST S	PLAN 0410527 BLOCK 1 LOT 1 2.75 ACRES	HUNT, DONNA E	REID, WILLIAM RONALD
1850 58 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 27 THAT PORTION OF THE SOUTH EAST QUARTER WHICH LIES NORTHEAST OF RAILWAY ON PLAN RY23 CONTAINING 13.4 HECTARES (33.1 ACRES) MORE OR LESS EXCEPTING THEREOUT: PLAN NUMBER HECTARES (ACRES) MORE O	RBK FARMS LTD	
4717 24 AVE S	DESCRIPTIVE PLAN 1111181 BLOCK 7 LOT 7	EVANGELICAL FREE CHURCH OF LETHBRIDGE	
1602 58 ST S	THE NORTH EAST QUARTER OF SECTION TWENTY SEVEN (27) IN TOWNSHIP EIGHT (8) RANGE TWENTY ONE (21) WEST OF THE FOURTH MERIDIAN CONTAINING ONE HUNDRED AND SIXTY (160) ACRES MORE OR LESS EXCEPTING THEREOUT PLAN NUMBER HECTARES (ACRES) RAILWA	DOMENIC LAND DEVELOPMENT CORP.	
709 1 AVE S	PLAN MAIN LINE, 8.705 MILES RIGHT OF WAY IN SEC1-9-22-4,2-9-22-4,3-9-22-4,6-9-21-4,35-8-22-4,36-8-22-4,33-8-21-4 & PLAN 8410666 BLK A & PLAN 8711578 BLK 2 & 5	CANADIAN PACIFIC RAILWAY	REAL ESTATE TAXATION
4610 58 ST S	PLAN 0613929 BLOCK 1 LOT 3	HIRONAKA, CLAYTON AKIRA	HIRONAKA, KIRSTEN ELLINGSON
4446 58 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 THE NORTHERLY 792 FEET OF THE SOUTHERLY 1658.25 FEET OF THE NORTH EAST QUARTER CONTAINING 19.4 HECTARES (48 ACRES) MORE OR LESS EXCEPTING THEREOUT: PLAN NUMBER HECTARES ACRES MORE OR LESS SUBDI	DOMENIC LAND DEVELOPMENT CORP.	
4406 58 ST S	PLAN 9910301 BLOCK 1 9.19 ACRES MORE OR LESS	TONE, JEAN-PAUL	TONE, DANIELLE
4304 58 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 THAT PORTION OF THE NORTH EAST QUARTER WHICH LIES NORTH OF THE SOUTHERLY 1658.25 FEET THROUGHOUT OF SAID QUARTER SECTION CONTAINING 23.92 HECTARES (59.5 ACRES) MORE OR LESS EXCEPTING THEREOUT: PLAN NUMB	RDFM HOLDINGS LTD..	
4820 58 ST S	THE NORTH EAST QUARTER OF SECTION FIFTEEN (15), IN TOWNSHIP EIGHT (8) RANGE TWENTY ONE (21) WEST OF THE FOURTH MERIDIAN, CONTAINING ONE HUNDRED AND SIXTY (160) ACRES, MORE OR LESS EXCEPTING: FIRSTLY: THE NORTHERLY SEVEN HUNDRED AND NINETY TWO (792) FEET	STEED, MERRILL D	STEED, JOY M

Civic Address	Legal Address	Land Owner 1	Land Owner 2
4220 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 THAT PORTION OF LEGAL SUBDIVISION 13 IN THE NORTH WEST QUARTER WHICH LIES TO THE SOUTH AND WEST OF THE ROADWAY ON PLAN 3187K CONTAINING 13.264 HECTARES (32.9) ACRES MORE OR LESS EXCEPTING THEREOUT: PLAN	BROVOLD, ALLAN	BROVOLD, LINDA
5205 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 THAT PORTION OF LEGAL SUBDIVISION 6 IN THE SOUTH WEST QUARTER WHICH LIES TO THE NORTH AND EAST OF THE ROADWAY ON PLAN 1025EZ CONTAINING 12.1 HECTARES (30 ACRES) MORE OR LESS	KUNZ, RICHARD	
5415 43 ST S	PLAN 0310216 BLOCK 1 LOT 3	LETHBRIDGE & DISTRICT EXHIBITION	
4310 24 AVE S	DESCRIPTIVE PLAN 0914974 BLOCK 12 LOT 10	1707588 ALBERTA LTD..	
5310 24 AVE S	PLAN 1457JK LOT 1	VERWOERD, JAMES DICK 1/2 INT	
5620 24 AVE S	PLAN 1457JK LOT 2	WIPF, DAVID	WIPF, LILLIAN
5730 24 AVE S	PLAN 1457JK LOT 3	WIPF, DAVID	WIPF, LILLIAN
2820 58 ST S	PLAN 1457JK LOT 4 (RESERVE)	MEHRER, MELVIN E	
3010 58 ST S	PLAN: 22-8-21-4 BLOCK: NE LOT: 160 ACS EXC: IRR189(6.60),3371EZ(7.38), 761JK(.57),1457JK(16.75); PTN OF N 172' S OF HWY R/W 3371EZ & W OF R/W 761JK(0.36 ACS); PTN OF W 40' S OF N 172'(2.27 ACS); PTN CANAL R/W 0311792 SO OF R/W IRR & S OF S BD	DOMENIC CONSTRUCTION LTD	
3115 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 THE NORTH HALF OF LEGAL SUBDIVISIONS 5 AND 6 IN THE SOUTH WEST QUARTER CONTAINING 16.2 HECTARES (40 ACRES) MORE OR LESS EXCEPTING THEREOUT THE NORTHERLY 50 FEET THROUGHOUT SAID LEGAL SUBDIVISIONS, AND THAT PORTIO	DOMENIC LAND DEVELOPMENT CORP	
3325 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 THE SOUTH HALF OF LEGAL SUBDIVISIONS 5 AND 6 IN THE SOUTH WEST QUARTER CONTAINING 16.2 HECTARES (40 ACRES) MORE OR LESS	DOMENIC LAND DEVELOPMENT CORP	
3605 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 THE NORTH HALF OF LEGAL SUBDIVISIONS 3 AND 4 IN THE SOUTH WEST QUARTER CONTAINING 16.2 HECTARES (40 ACRES) MORE OR LESS	ANDERSON, GORDON FAIRBANKS	
4025 43 ST S	PLAN 0111356 BLOCK 2 LOT 1 (8.43 ACRES)	GWATKIN, RICHARD	GWATKIN, CAROL
4215 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 THAT PORTION OF THE NORTH WEST QUARTER WHICH LIES EAST OF ROAD PLAN 3187K CONTAINING 33.8 HECTARES (83.52 ACRES) MORE OR LESS EXCEPTING THEREOUT: PLAN NUMBER HECTARES (ACRES) MORE OR LESS SU	GWATKIN FARMS LTD	
3000 58 ST S	PLAN 0311792 CANAL RIGHT OF WAY WITHIN MERIDIAN 4, RANGE 21, TOWNSHIP 8 SECTION 22 QUARTER NORTH EAST THAT PORTION LYING SOUTH OF THE SOUTHERLY BOUNDARY OF RIGHT OF WAY PLAN IRR 189 AND SOUTH OF THE SOUTHERLY BOUNDARY OF RIGHT OF WAY PLAN 761 JK EXCEPTIN	ST MARY RIVER IRRIGATION DISTRICT	
3125 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 THE NORTHERLY 50 FEET THROUGHOUT LEGAL SUBDIVISIONS 5 AND 6 AND THAT PORTION OF THE EASTERLY 50 FEET OF THE NORTH HALF OF LEGAL SUBDIVISION 6 WHICH LIES TO THE SOUTH OF THE NORTHERLY 50 FEET OF THE SAID LEGAL SUB	DOMENIC LAND DEVELOPMENT CORP	
3110 58 ST S	PLAN NOPLAN MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 QUARTER SE	DOMENIC CONSTRUCTION LTD	
4900 24 AVE S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 FIRSTLY: THAT PORTION OF THE NORTHERLY 172 FEET OF THE NORTH EAST QUARTER WHICH LIES SOUTH OF THE HIGHWAY RIGHT OF WAY ON PLAN 3371EZ AND WEST OF THE CANAL RIGHT OF WAY ON PLAN 761JK, CONTAINING .146 HECTARES (0	DOMENIC LAND DEVELOPMENT CORP	
3927 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 22 THE SOUTH HALF OF LEGAL SUBDIVISIONS 3 AND 4 IN THE SOUTH WEST QUARTER CONTAINING 16.2 HECTARES (40 ACRES) MORE OR LESS EXCEPTING THEREOUT: SUBDIVISION 1512240 4.239 (10.47)	DOMENIC LAND DEVELOPMENT CORP	
5503 43 ST S	PLAN 0310216 BLOCK 1 LOT 2 4.67 ACRES	HEGGIE, SHAWN BRUCE	HEGGIE, JONI L
5920 43 ST S	PLAN 9210487 BLOCK 1 LOT 1	KUTSCH, DEVON RUDY	KUTSCH, JULIE ANNE
5310 43 ST S	MERIDIAN 4 RANGE 21 TOWNSHIP 8 SECTION 15 LEGAL SUBDIVISION 3 AND LEGAL SUBDIVISION 6 IN THE SOUTH WEST QUARTER CONTAINING 32.4 HECTARES (80 ACRES) MORE OR LESS EXCEPTING THEREOUT: (A) PLAN NUMBER HECTARES ACRES ROAD DIVERSION	DANIELS, JACOB ALVIN	DANIELS, CATHERINE ROXANNE
4045 60 AVE S	MER 4 RNG 21 TWNSHP 8 SEC 15 LEGAL SUB 4 AND 5 IN SW 1/4 EXCEPTING THEREOUT: FIRST: FRM SUB 4 THAT PTN WHICH LIES TO THE S OF A LINE DRAWN PARALLEL WITH 40 FT PERPENDICULARLY DISTANT N FROM THE S BOUNDARY OF SUB 4 WHICH LIES OUTSIDE THE LIMITS OF THE RD	1886511 ALBERTA LTD.	



CITY OF  
*Lethbridge*

